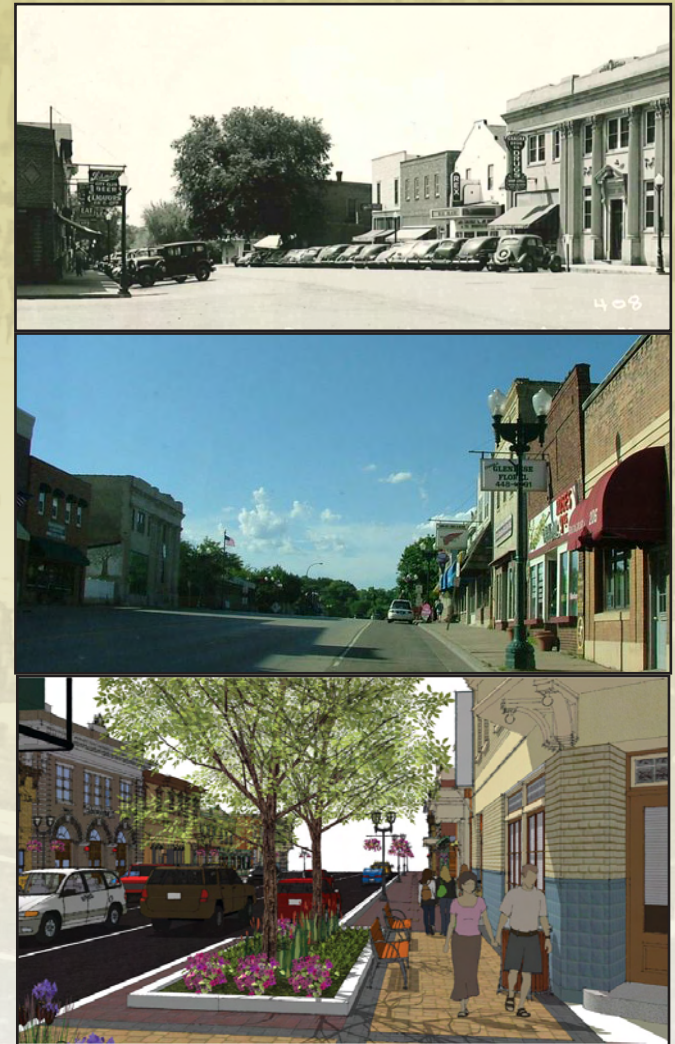


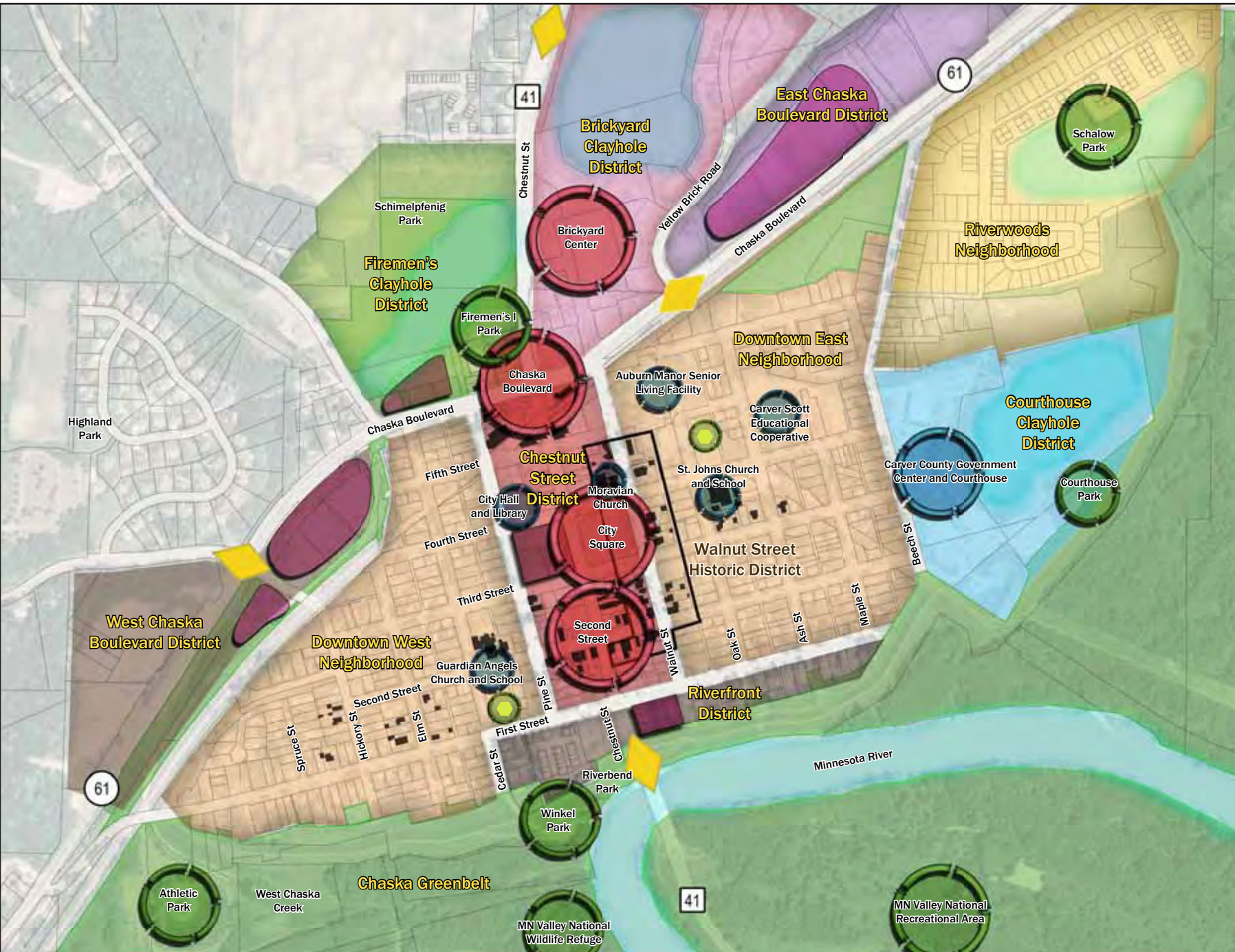
## CHAPTER 7: downtown plan for the future

Building upon the future vision of Downtown Chaska that encompasses multiple districts and neighborhoods, the Downtown Plan for the Future envisions downtown as home to a wide variety of destinations. In order to attract people downtown, there needs to be a concentration or critical mass of destinations. These destinations need to be woven together and complement each other in order to restore downtown as the community's main gathering place.

As a traditional small town, Chaska's downtown should be centered on City Square and "Main Street" – Chestnut Street - as its heart and spine. The Chestnut Street district should function as a series of "Main Street" destinations from the southern riverfront gateway to the historic Chestnut & Second St commercial node to the historic City Square node to the City Plaza node to the Chaska Boulevard node to the northern downtown gateways of Fireman's Clayhole district and Brickyard Clayhole district. Beyond the downtown core are major community institutions, including the Carver County Government Center, Guardian Angels Church & School, St. John's Church & School, Carver-Scott Educational Cooperative School, Moravian Church and Auburn Manor senior living campus. As the centerpiece of downtown, the Chestnut Street district contains a wealth of historic buildings that should be sensitively preserved, reused, and celebrated as the key to retaining and enhancing downtown Chaska's unique sense of place.

Downtown will also become a destination for a wealth of recreational opportunities. As one of the most visible and accessible segments of the Chaska Greenbelt, the downtown greenbelt offers significant potential for creating a major natural and recreational amenity right next to downtown. Improvements to Athletic Park, Winkel Park, Riverbend Park, the levee trail/open space system, and the MN Valley National Wildlife Refuge will build a one-of-a-kind downtown greenbelt for Chaska residents and





- Legend:**
- Open Space Destination
  - Main Street Destination
  - Community Institution Destination
  - Catalyst Redevelopment Site
  - Downtown Gateway
  - Future Neighborhood Parks
  - Designated Historic Buildings

Vision:  
Downtown  
Destinations



Figure 7-1. Vision of Downtown Destinations.

visitors. Fireman’s Park will be redesigned and enhanced as the signature downtown community park that integrates the site’s natural features, including Fireman’s Clayhole, with its heritage as a former brickyard and strategic location in the downtown gateway area. For downtown residents and community institutions, high quality neighborhood parks will be added to both neighborhoods. Development of a more complete downtown trail network, using the levee as well as streets, will create better recreational trail connections within downtown and links to the regional trail network.

Downtown’s streets are in poor condition and are all planned to be reconstructed over the next ten years. Redesigning and reconstructing the streets to create a much more attractive, comfortable and safer environment for all users will attract more residents and visitors to downtown, whether driving, walking or biking. Working with MN DOT, it is critical that Chestnut St be redesigned as a more traditional and multi-functional “Main Street” that balances the needs of vehicles, pedestrians, and businesses.

## A. Built Environment

Downtown’s future built environment should celebrate its historic and traditional character yet embrace additions and improvements that enhance the functionality, attractiveness, and experience of downtown to meet the needs of today’s economy and lifestyles. Although downtown contains some potential redevelopment sites, it is not a redevelopment area in general.

### Minnesota’s Brick City

Downtown Chaska’s wealth of remaining historic buildings and sites is one of its strongest assets. Downtown’s historic buildings and sites include a broad range of building types, including mixed-use, commercial, industrial, churches, hotel, train depot, livery stables, residential (stately mansions, single-family homes, and rowhouses), ball park, and clayhole lakes (former brickyard areas). These historic buildings and sites create a very strong sense of place in downtown. In particular, the concentration of Chaska brick buildings in downtown makes it a unique place. Historic Chaska brick commercial/mixed-use buildings are clustered along Second St and Chestnut St while former industrial buildings are clustered along the recently abandoned rail line. While historic Chaska brick residences line Walnut

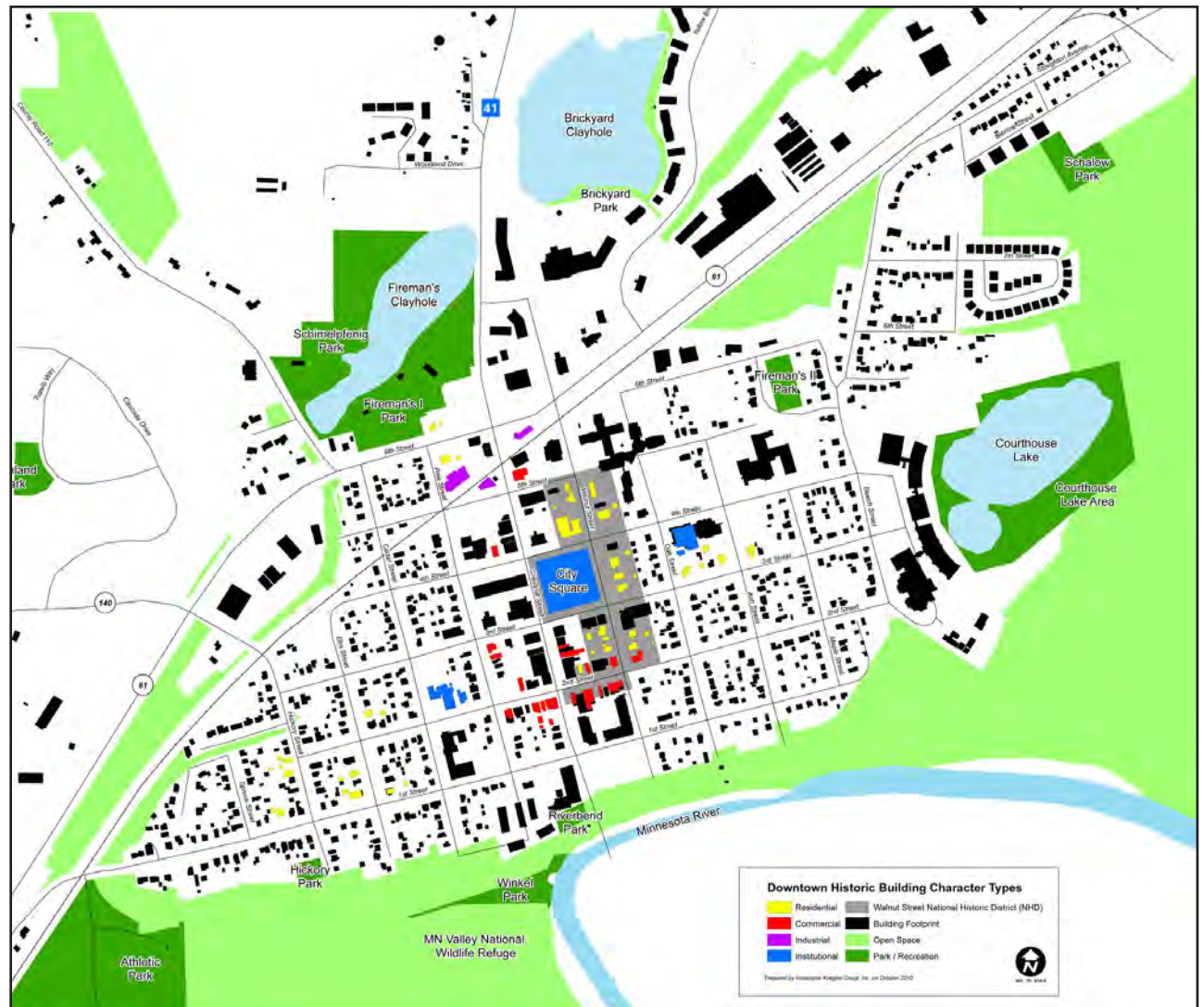


Figure 7-2. Map of downtown built environment including historic buildings by character type.

St, hence the designation of the Walnut Street National Historic District, they are also scattered along most other downtown streets.

In 1996, the City Council adopted a resolution recognizing and declaring the City of Chaska as “Minnesota’s Brick City”. Since, literally, no other downtown in the world has this unique Chaska brick building character, any future changes in downtown should complement and enhance the unique character of its Chaska brick buildings. Revitalization of downtown Chaska should capitalize on downtown’s unique potential as “Minnesota’s Brick City”. Preservation and enhancement of downtown’s historic character, including the Chaska brick building character, is especially important along Chestnut St, as Chaska’s “Main Street”, Second St, Walnut St, and surrounding City Square. Since these four areas have the strongest historic character, they offer the greatest opportunities for revitalization initiatives.

Downtown revitalization is two-pronged in regard to buildings: redevelopment of vacant/underutilized sites and reinvestment in historic buildings. A key initiative for restoring and expanding downtown’s unique character as “Minnesota’s Brick City” is to spur on rehabilitation of existing historic buildings in the Chestnut St district. A number of historic preservation success stories have occurred in downtown over the years. Since the true strength of downtown’s historic character is related more to the remaining clusters or streetscapes of historic buildings, rather than individual buildings, it is important that all buildings be restored on



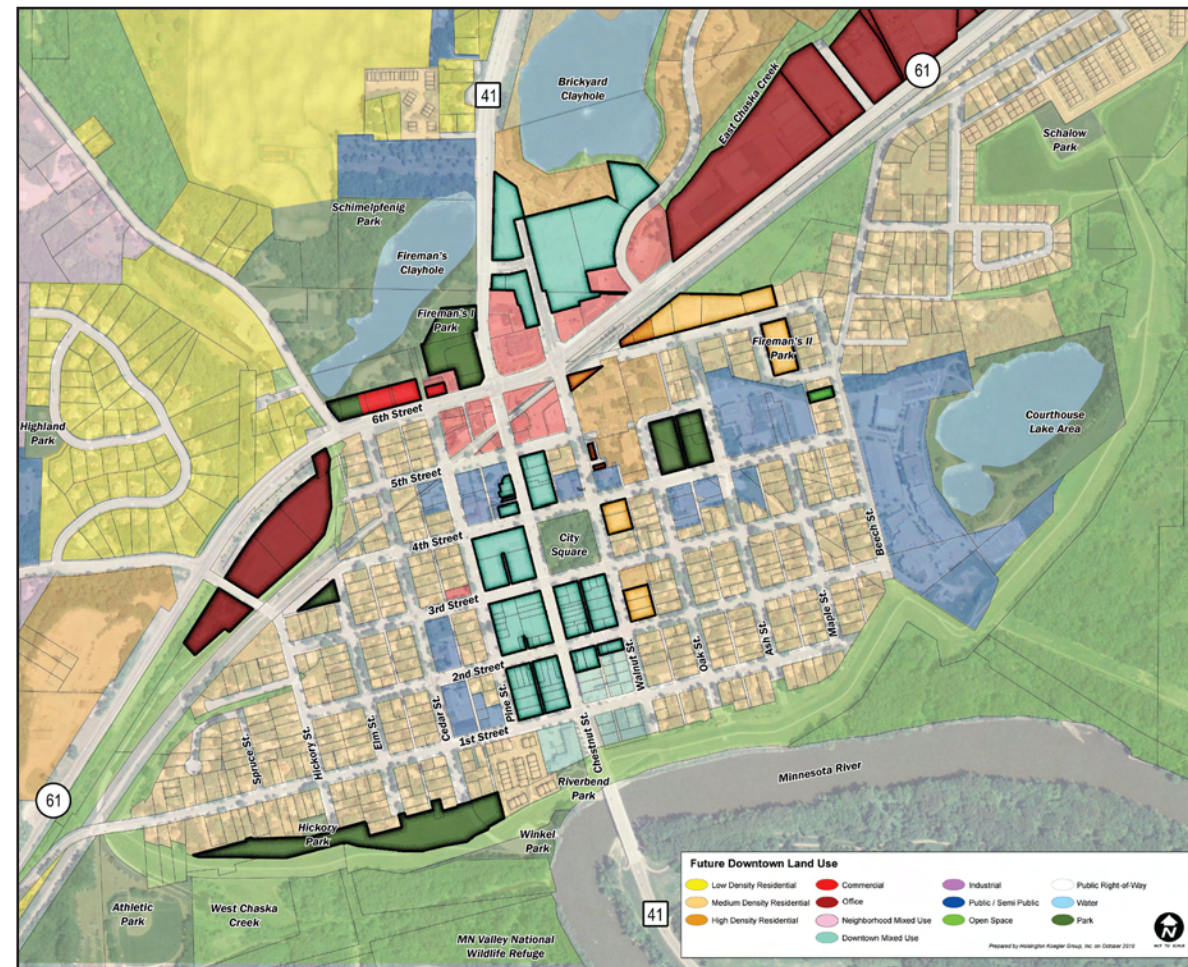
Figure 7-3. The Potential Revitalization Opportunities map above shows vacant sites, as well as potential sites for redevelopment, property improvements, or historic building renovations.

a street with historic character. Downtown revitalization should provide for greater interpretation and connectivity between the variety of historic areas in downtown to create a more legible and intriguing historic context for downtown activities. For redevelopment sites, it is critical that new buildings be designed to complement the character of historic downtown buildings, particularly in the Chestnut Street district. Key elements of historic commercial/mixed-use buildings in downtown include Chaska brick exterior, two stories, modest architecture, large storefront windows, and second-story balconies.

## Traditional Small Town Character

Downtown Chaska’s traditional small town heritage and character should be preserved and enhanced as part of future revitalization initiatives. From a city design perspective, the key elements of this traditional small town character include City Square, “Main Street”, river town, a regular street grid oriented to the river, alleys, sidewalks both sides of streets, broad mix of land uses, primarily two-story mixed-use buildings on “Main Street”, narrow side-by-side buildings with large window storefronts, on-street diagonal parking, and a variety of older homes on tree-lined residential streets.

To strengthen the traditional small town character of downtown, some key revitalization initiatives will be to increase street network connections, particularly in the northeast quadrant, maximize on-street parking options and limit off-street parking lots that front onto streets, ensure heights and widths of new “Main Street” buildings fit with the traditional downtown scale, and place new buildings up to the sidewalk. Future parking strategies should support economic development in downtown, however, additional off-street surface parking options should be discouraged from harming the traditional character of side-by-side buildings along downtown core streets.



## Future Land Use Patterns

If the goal is to create a great downtown, it’s not enough to have a single land use that dominates the downtown area. The future land use plan for downtown should serve as a guide for increasing downtown’s mix of land uses, including commercial, office, civic, residential, and park/open space. The Chestnut St district, which is the downtown core, should become a downtown mixed-use district that is dominated by commercial storefronts at street level with other uses above street level. Along Chaska Boulevard/Cty Rd 61, commercial land uses are envisioned to remain as this is a major gateway area for downtown.

Large downtown edge sites along Chaska Boulevard/Cty Rd 61 that are vacant/underutilized should be redeveloped with commercial/industrial land uses that will not directly compete with existing and future retail/restaurant land uses in the downtown core.

The Riverfront District should be guided for High Density Residential as these four half-blocks offer the only future potential for developing new housing that could take advantage of river views and connections. Higher Density Residential is also appropriate for the large potential redevelopment site on the west side of Chaska Boulevard/Cty Rd 61.

Figure 7-4. Map shows recommended future land uses for the downtown area with recommended land use changes outlined in black.

## Convenient Parking

To support the success of existing downtown businesses and attract new businesses, downtown parking needs to be managed wisely for ensuring adequate quantity, convenience and quality of parking areas. Chaska's traditional downtown character of side-by-side building storefronts located up to the sidewalk is primarily reliant on street parking rather than off-street parking for each building. Since the community's goal is to preserve downtown's historic character, future parking will continue to be primarily reliant on public parking. Downtown's future parking needs will be provided through a variety of types: on-street diagonal spaces, on-street parallel spaces, off-street surface lots (private, shared, and public), and a potential municipal parking ramp. While future off-street parking lots could be options, the location, size and design of off-street parking lots needs to be sensitive to the preservation of downtown's historic character of streets enclosed with building storefronts up to the sidewalk.

A key strategy for a successful downtown parking program is to have a Downtown Parking Management Plan that is used, maintained and enforced. The parking plan should include designating on-street parking areas for customers/clients vs. business owners/employees, improving and expanding short term (two-hour) parking areas, increasing the quantity of on-street parking spaces by converting some parallel parking areas to diagonal spaces, setting up shared parking agreements between businesses, developing public off-street parking lots, improving signage for parking, and negotiating with MN DOT to redesign Chestnut Street (MN Hwy 41) with on-street parking on both sides.



Figure 7-5. Downtown Parking.

## Catalyst Redevelopment Sites

Opportunity redevelopment sites were identified within the downtown area. These sites are vacant, owned by the City of Chaska, underutilized sites, or contain uses and structures that do not fit or support the context and development pattern of historic downtown Chaska. While the downtown area contains several opportunity sites as shown in Figure 7-3, three sites were identified for further study. Each site represents a significant opportunity for visible and positive change that can catalyze new interest and the revitalization of downtown Chaska. These catalyst sites, chosen for further study, include Fireman’s Clayhole District (the north gateway into downtown), the Riverfront District (the south gateway into downtown), and City Square West, which is the block west of City Square in the heart of downtown.

Several revitalization scenarios were created for each catalyst site. Each scenario explores a theme, land use options, open space and parking strategies, and analyzes redevelopment potential. Revitalization scenarios explore ways to create new businesses, housing, civic institutions, parks and gathering spaces for downtown Chaska. They explore options for additional public parking facilities, restoring historic patterns of development, preserving community identity and enhancing a sense of place unique to downtown Chaska. The following provides a more detailed description for each scenario prepared for the three catalyst redevelopment sites.

### ***Fireman’s Clayhole District***

Fireman’s Clayhole District encompasses the quadrant northwest of the intersection of Chestnut Street/MN Highway 41 and Chaska Boulevard/County Road 61 that surrounds

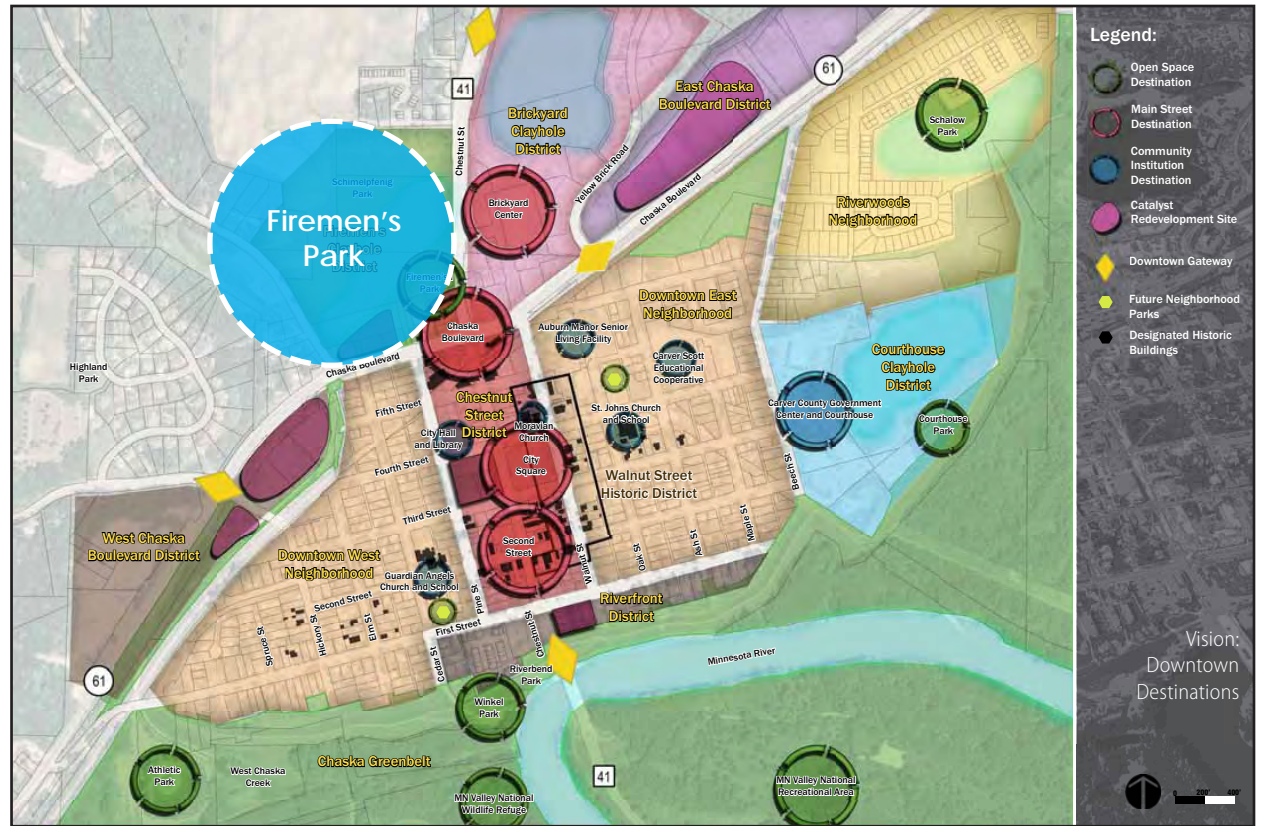


Figure 7-6. Fireman’s Clayhole District catalyst redevelopment site.

Fireman’s Clayhole Lake. This area was the location of one of the major brickyards in Chaska and the clayhole lake is a remnant of this previous industry. Fireman’s Park rests on land donated to the City of Chaska in 1961 by Charles Klein for use as a public park. Many of the park’s features were originally built by the Chaska Fire Department. The park’s historical significance, visibility as a gateway into downtown Chaska, convenient access off Chaska Boulevard, and public ownership make it an excellent candidate as a site to catalyze revitalization and enhance community identity for downtown Chaska.

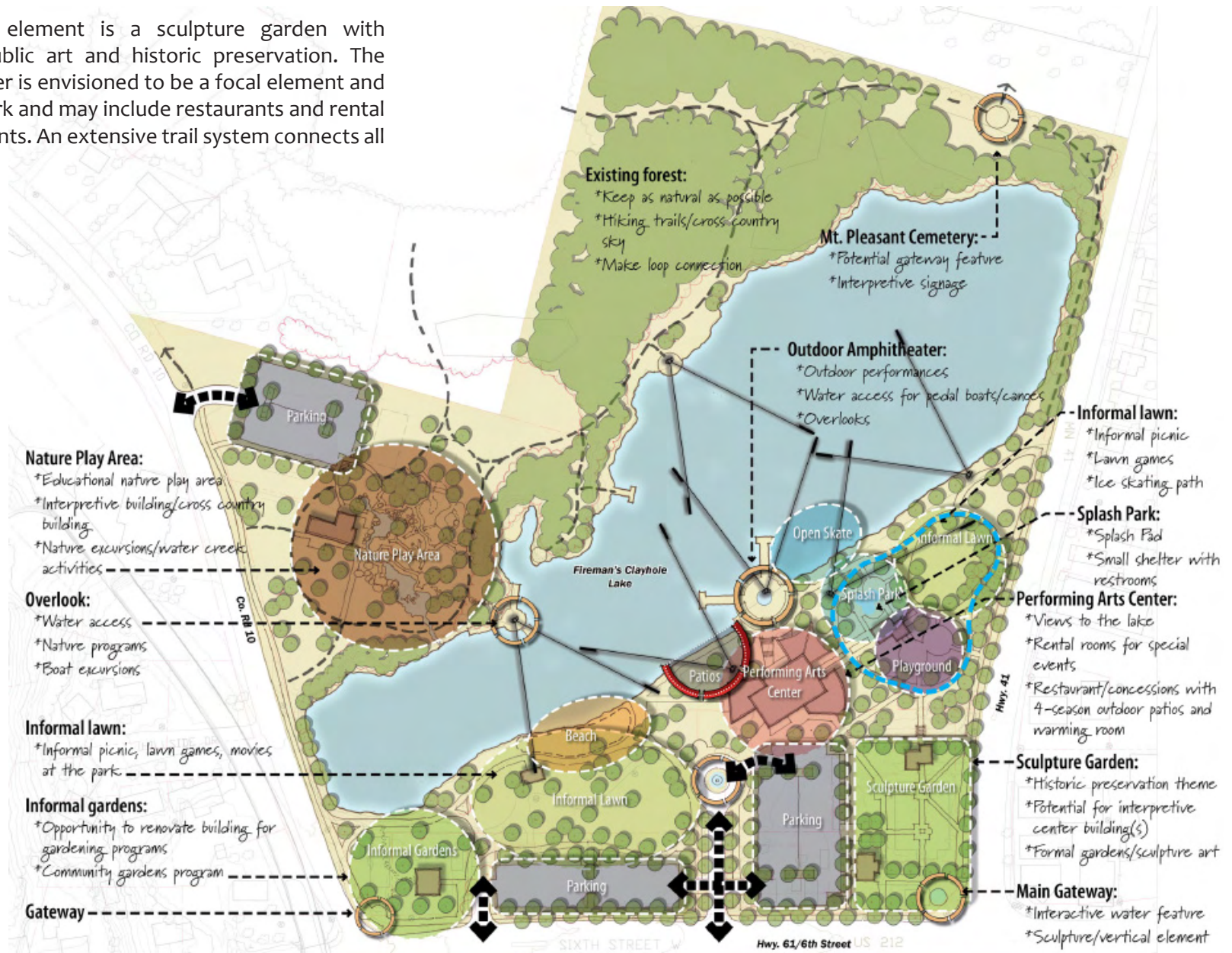
The site’s future identity is envisioned as an expanded and enhanced signature downtown community park that serves as a downtown gateway, visual landmark, gathering place, trailhead and community destination with strong connections to Chestnut Street and the historic downtown district. The site is envisioned to include complementary commercial uses, preferably restaurants or entertainment, that have good highway visibility and a relationship to the park and lake amenities. The character of future development should complement the historic context of downtown Chaska and celebrate the importance of this site to the City’s brickmaking history.

## Fireman's Clayhole District - Scenario A

Scenario A features a performing arts center on the lake, a splash park, sculpture garden, nature-based play area, playground, beach, gardens, patios and open lawn areas. Parking is provided on-site.

The main gateway element is a sculpture garden with opportunities for public art and historic preservation. The performing arts center is envisioned to be a focal element and destination of the park and may include restaurants and rental rooms for special events. An extensive trail system connects all park destinations.

Figure 7-7. Redevelopment scenario A for Fireman's Clayhole District catalyst redevelopment site.

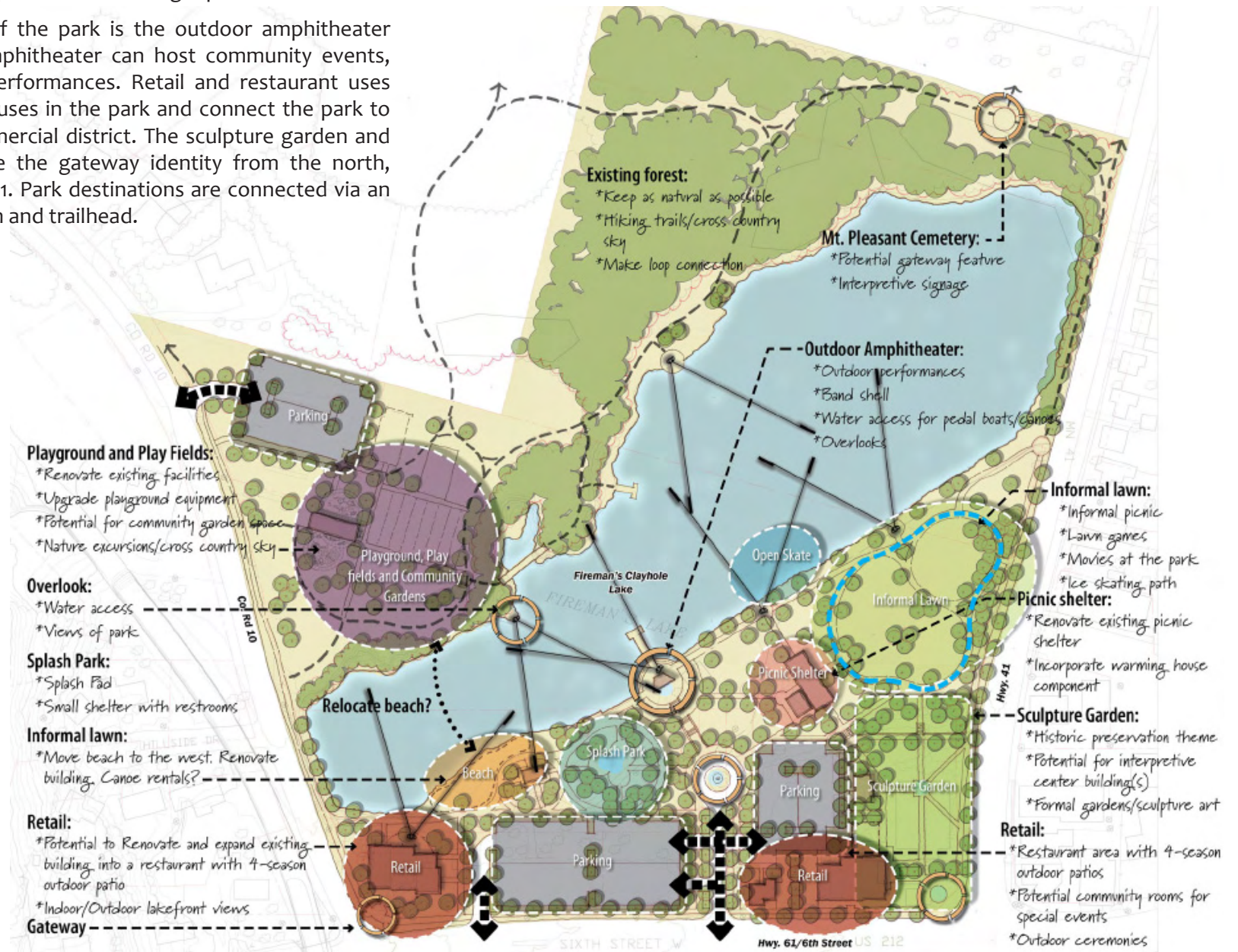


## Fireman's Clayhole District - Scenario B

Scenario B features a renovated picnic shelter, a retail/restaurant node along County Road 61, splash park, sculpture garden, beach, playground, play fields, community gardens, and a community amphitheater. Parking is provided on-site.

The focal element of the park is the outdoor amphitheater on the lake. The amphitheater can host community events, plays and musical performances. Retail and restaurant uses should complement uses in the park and connect the park to the downtown commercial district. The sculpture garden and informal lawn create the gateway identity from the north, along MN Highway 41. Park destinations are connected via an extensive trail system and trailhead.

Figure 7-8. Redevelopment scenario B for Fireman's Clayhole District catalyst redevelopment site.

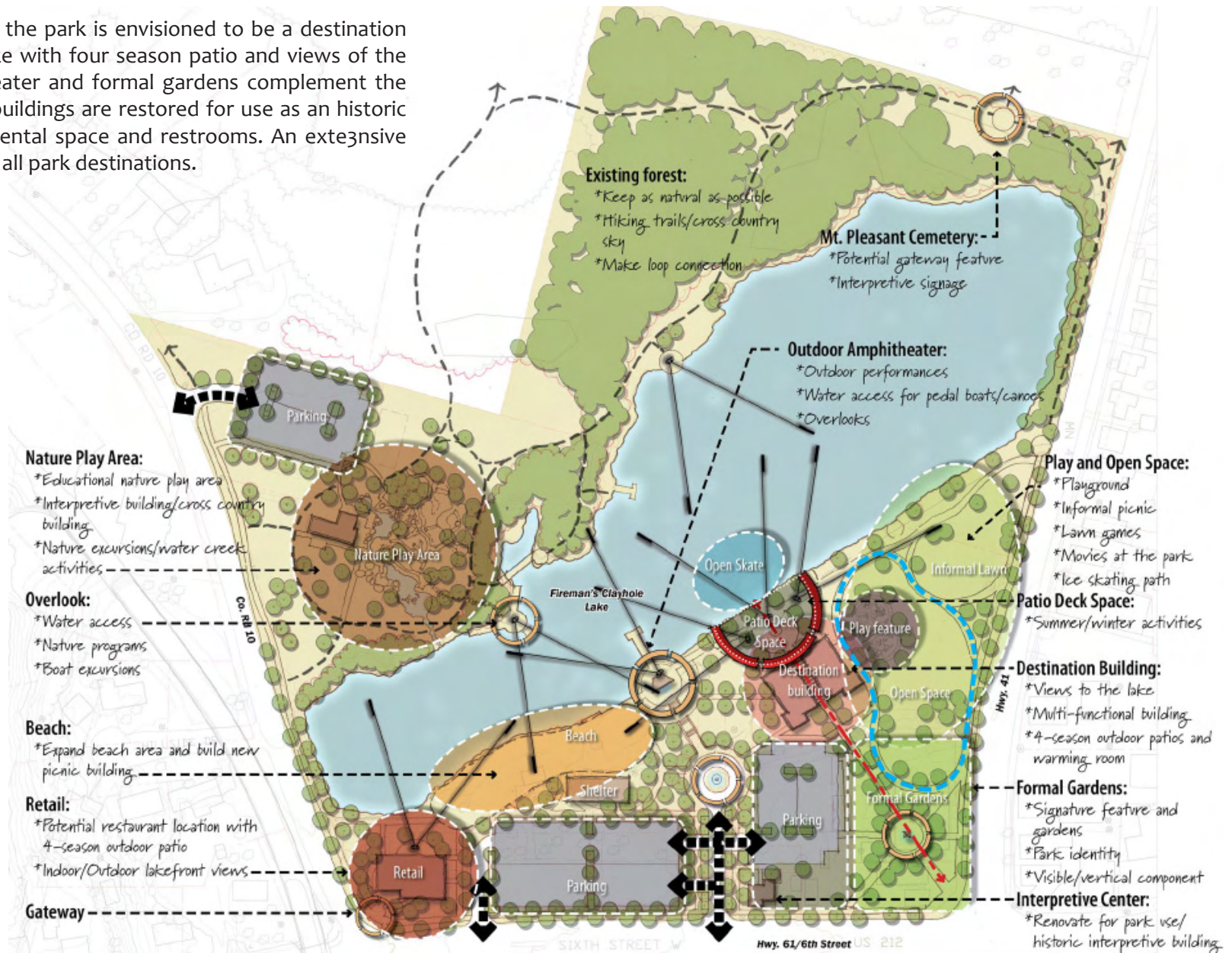


## Fireman's Clayhole District - Scenario C

Scenario C features a multi-use park building, interpretive center, outdoor amphitheater, large beach, nature-based play area, play features, formal gardens open lawns and outdoor patios on the lake. Parking is provided on-site.

The focal element of the park is envisioned to be a destination restaurant on the lake with four season patio and views of the lake. An outdoor theater and formal gardens complement the restaurant. Existing buildings are restored for use as an historic interpretive center, rental space and restrooms. An extensive trail system connects all park destinations.

Figure 7-9. Redevelopment scenario C for Fireman's Clayhole District catalyst redevelopment site.



### City Square West

The site entitled “City Square West” lies directly to the west of City Square Park, Block 27, located between Chestnut and Pine Streets, and Third and Fourth Streets. For several decades, the site was host to the Carver County Courthouse and County Library, a two-story, Italianate-style building that proudly fronted City Square Park, until it was replaced in 1966. Today the site contains a bank, post office, and a mix of various retail uses including restaurants, laundry mat and surface parking lots. The sites buildings are primarily one-story structures, oriented more toward the automobile (parking lots and drive-thru facilities) than the pedestrian (streets and sidewalks). This pattern of development is consistent with their period of construction but does not reflect the character or development patterns of historic downtown Chaska.

While the site contains shops and services that provide a positive contribution to downtown Chaska, the existing structures don’t contribute to the character, scale or identity of downtown Chaska. Nor do they contribute to the vitality of Chestnut Street as a vibrant, pedestrian-oriented commercial street. The opportunity exists at City Square West to create a mixed-use development more consistent with the character, scale and materials of downtown Chaska.

City Square West is envisioned as a mixed-use block containing retail, multi-family residential, civic and public parking facilities. Traditional storefront buildings that complement downtown’s historic character and scale are recommended. Buildings should orient themselves to the street to create interesting and pedestrian-friendly sidewalks surrounding the development. Every effort should be made to accommodate existing businesses in all redevelopment scenarios.



Figure 7-10. City Square West catalyst redevelopment site.

## City Square West - Scenario A

Scenario A features multi-story mixed-use buildings, parking ramp, and “paseo” treatment of alley. The buildings front the surrounding streets and register with the historic scale of downtown Chaska, creating a more interesting and pedestrian-friendly streetscape. The uses fronting Chestnut Street include a small library, bank, post office and ground level retail/restaurants. Residential uses are planned above. The scale of the residential uses fronting Pine Street are more compatible with the single family residential across the street (townhomes). The center of the block features a parking ramp and surface parking courtyard. The parking ramp and courtyard are envisioned to accommodate the uses on-site as well as public parking needs in other segments of downtown Chaska. The following is a breakdown of the proposed development program.

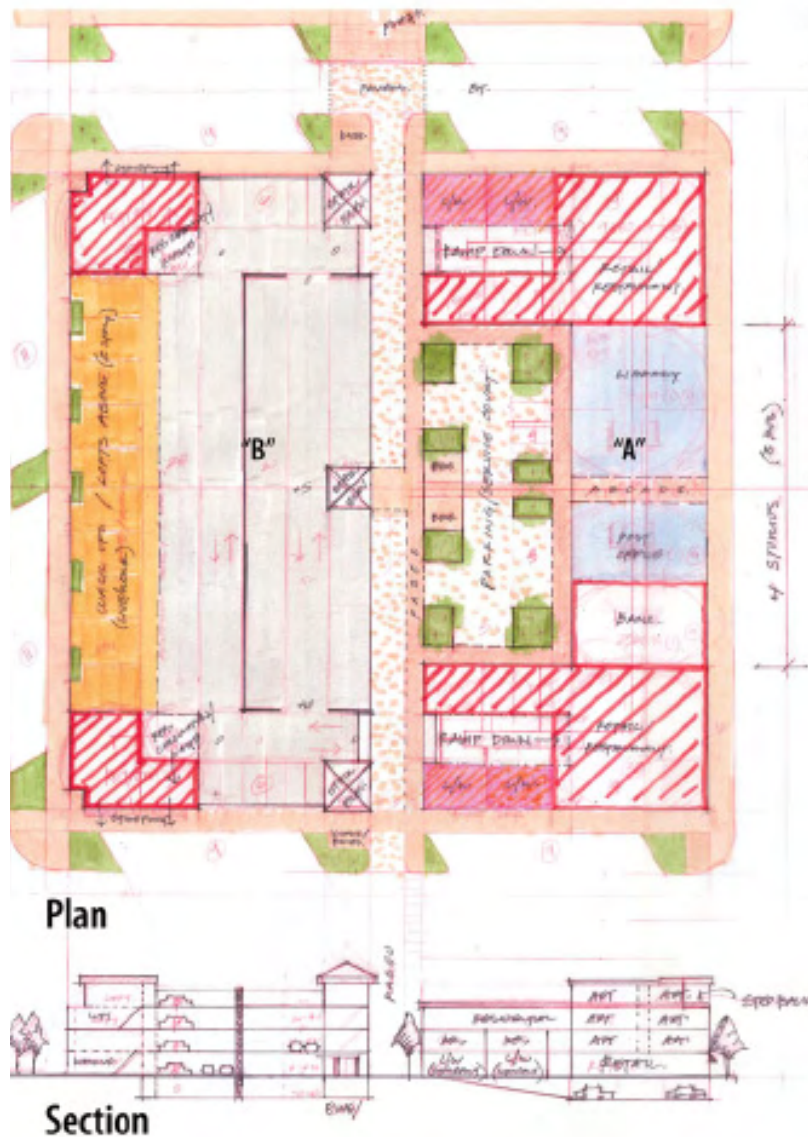
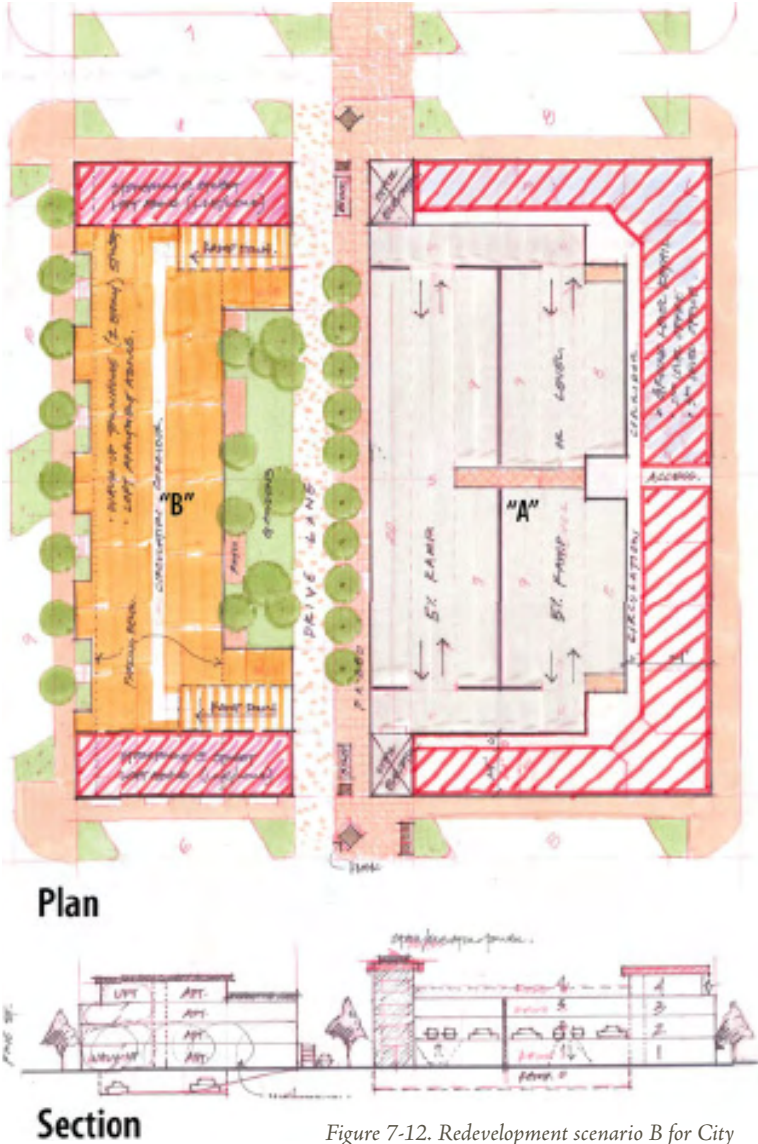


Figure 7-11. Redevelopment scenario A for City Square West catalyst redevelopment site.

Building A		Parking requirement = 209 spaces
Ground Story:	Retail/Restaurant (14,600 SF) Library (4,550 SF) Post Office (2,250 SF) Bank (2,250 SF) Live/work (6 units)	
2nd Story:	Residential (34 units)	
3rd Story:	Residential (34 units)	
4th Story:	Residential (12 units)	
Underground Parking:	56 spaces	
Courtyard Parking:	14 spaces	
Parking Ramp (part of building B)		
Level 0 (below grade):	70 spaces	
Level 1:	70 spaces	
Level 2:	70 spaces	
Level 3:	70 spaces	
Level 4:	70 spaces	
Total spaces:	280 spaces	
Building B		Parking requirement = 46 spaces
Ground Story:	Retail/Restaurant (4,000 SF)	
Ground/2nd Story:	Residential (10 units)	
3rd/4th Story:	Residential (10 units)	
Surplus parking:	25 spaces (ramp) 58 spaces (on-street) 14 spaces (courtyard)	

**City Square West - Scenario B**

Scenario B features multi-story mixed-use buildings, parking ramp, and a shifted “paseo” drive lane. The buildings front the surrounding streets and register with the historic scale of downtown Chaska, creating a more interesting and pedestrian-friendly streetscape. The uses fronting Chestnut Street include ground level retail and restaurants. Office uses are planned above the retail. The building facing Pine Street is primarily residential, however, includes ground level retail facing 3rd and 4th Streets. The scale of the residential uses fronting Pine Street are more compatible with the single family residential across the street (townhomes). The center of the block features a parking ramp and a small planted courtyard. The parking ramp is envisioned to accommodate the uses on-site as well as public parking needs in other segments of downtown Chaska. The following is a breakdown of the proposed development program.

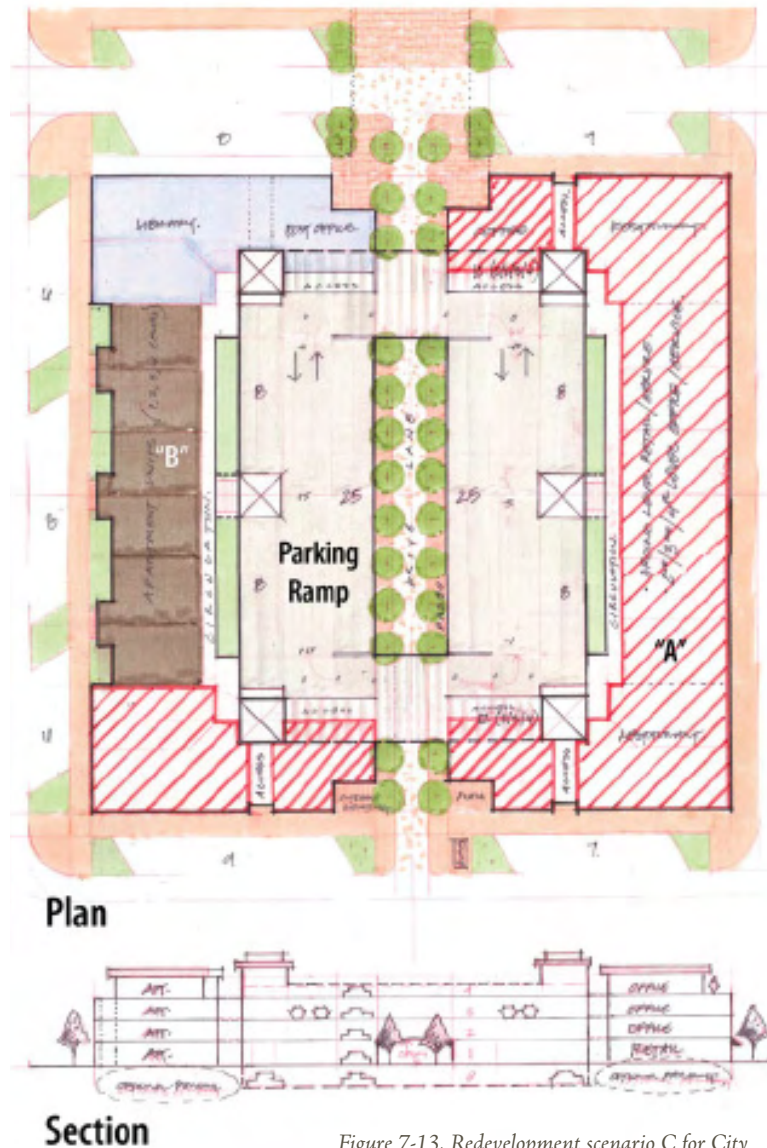


<b>Building A</b>		Parking requirement = 172 spaces
Ground Story:	Retail/Restaurant (15,100 SF)	
2nd Story:	Office (15,100 SF)	
3rd Story:	Office (15,100 SF)	
4th Story:	Office (15,100 SF)	
<b>Parking Ramp (part of building A)</b>		
Level 0 (below grade):	88 spaces	
Level 1:	72 spaces	
Level 2:	72 spaces	
Level 3:	72 spaces	
Level 4:	72 spaces	
Total spaces:	376 spaces	
<b>Building B</b>		Parking requirement = 114 spaces
Ground Story:	Retail/Restaurant (6,000 SF) Live/Work (8 units) Residential (14 units)	
2nd Story:	Residential (14 units)	
3rd Story:	Residential (12 units)	
4th Story:	Residential (12 units)	
Underground Parking:	54 spaces	
Surplus parking:	144 spaces (ramp)	

Figure 7-12. Redevelopment scenario B for City Square West catalyst redevelopment site.

## City Square West - Scenario C

Scenario C features multi-story mixed-use buildings and a detached central parking ramp, split by “paseo” treatment of alley. The buildings front the surrounding streets and register with the historic scale of downtown Chaska, creating a more interesting and pedestrian-friendly streetscape. The uses fronting Chestnut Street include ground level retail and restaurants. Office uses are planned above. The building facing Pine Street is also mixed-use but the scale of the residential uses fronting Pine Street are more compatible with the single family residential across the street (townhomes). The center of the block features a detached parking ramp. The parking ramp is envisioned to accommodate the uses on-site as well as public parking needs in other segments of downtown Chaska. The following is a breakdown of the proposed development program.

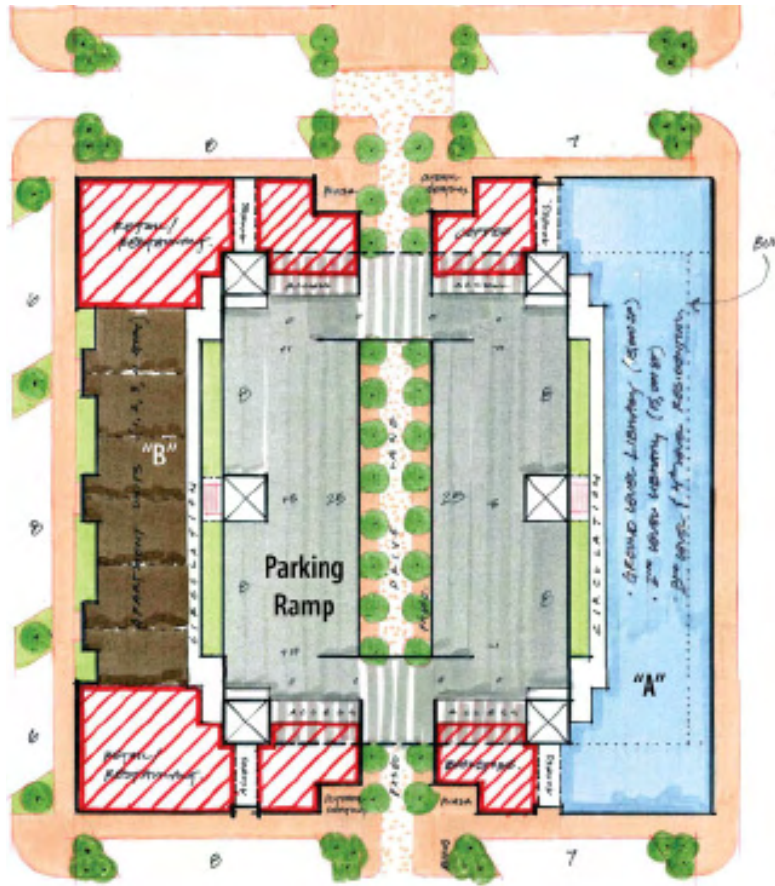


Building A		Parking requirement = 257 spaces
Ground Story:	Retail/Restaurant (20,500 SF)	
2nd Story:	Office (19,900 SF)	
3rd Story:	Office (19,900 SF)	
4th Story:	Office (10,400 SF)	
Parking Ramp		
Level 0 (below grade):	106 spaces	
Level 1:	82 spaces	
Level 2:	106 spaces	
Level 3:	106 spaces	
Level 4:	106 spaces	
Total spaces:	506 spaces	
Building B		Parking requirement = 103 spaces
Ground Story:	Retail/Restaurant (6,040 SF) Library (4,240 SF) Post Office (1,800 SF) Residential (6 units)	
2nd Story:	Residential (14 units)	
3rd Story:	Residential (14 units)	
4th Story:	Residential (6 units)	
Surplus parking:	146 spaces (ramp) 50 spaces (on-street)	

Figure 7-13. Redevelopment scenario C for City Square West catalyst redevelopment site.

### City Square West - Scenario D

Scenario D features multi-story mixed-use buildings and a detached central parking ramp, split by “paseo” treatment of alley. The buildings front the surrounding streets and register with the historic scale of downtown Chaska, creating a more interesting and pedestrian-friendly streetscape. The uses fronting Chestnut Street include ground level retail and restaurants. Office uses are planned above. The building facing Pine Street is also mixed-use but the scale of the residential uses fronting Pine Street are more compatible with the single family residential across the street (townhomes). The center of the block features a detached parking ramp. The parking ramp is envisioned to accommodate the uses on-site as well as public parking needs in other segments of downtown Chaska. The following is a breakdown of the proposed development program.



**Section**

Figure 7-14. Redevelopment scenario D for City Square West catalyst redevelopment site.

Building A		Parking requirement = 180 spaces
Ground Story:	Retail/Restaurant (3,800 SF) Library (15,500 SF)	
2nd Story:	Library (15,500 SF) Residential (4 units)	
3rd Story:	Residential (24 units)	
4th Story:	Residential (12 units)	
Parking Ramp		
Level 0 (below grade):	106 spaces	
Level 1:	82 spaces	
Level 2:	106 spaces	
Level 3:	106 spaces	
Level 4:	106 spaces	
Total spaces:	506 spaces	
Building B		Parking requirement = 116 spaces
Ground Story:	Retail/Restaurant (12,200 SF) Residential (6 units)	
2nd Story:	Residential (24 units)	
3rd	Residential (24 units)	
4th Story:	Residential (12 units)	
Surplus parking:	210 spaces (ramp) 50 spaces (on-street)	

## Riverfront District

The Riverfront District catalyst site includes Blocks 50-53, located at the south gateway into downtown Chaska. Block 53, located at the intersection of Chestnut and First Streets, is vacant and has been the subject of prior redevelopment proposals including multi-story residential buildings. This block should be the highest priority in the district as it is a vacant and very significant site in the context of downtown Chaska. It presents a tremendous opportunity to celebrate community identity and create a positive gateway into downtown at the river's edge.

Blocks 50, 51 and 52 contain single-family houses which are physically and visually separated from the river by the levee. These blocks should be considered for future redevelopment if and when additional housing density becomes desired downtown and other sites have been exhausted. These blocks offer the opportunity to create more downtown residential density at the river's edge and create value by providing views of the river in taller structures.

The Riverfront District's future identity should be one of riverfront housing in higher density residential buildings with convenient access to downtown amenities and the river corridor. Riverfront housing options should include townhouses, condos and apartments that are oriented to the river. New housing should provide river views and optimize the extensive park, trail and open space system along the river. Mixed-use development would be appropriate at the intersection of Chestnut and First Streets as the gateway corner to the Chestnut Street commercial district. The district should be designed to provide public access points to the levee trail and connected open spaces via the levee lanes.

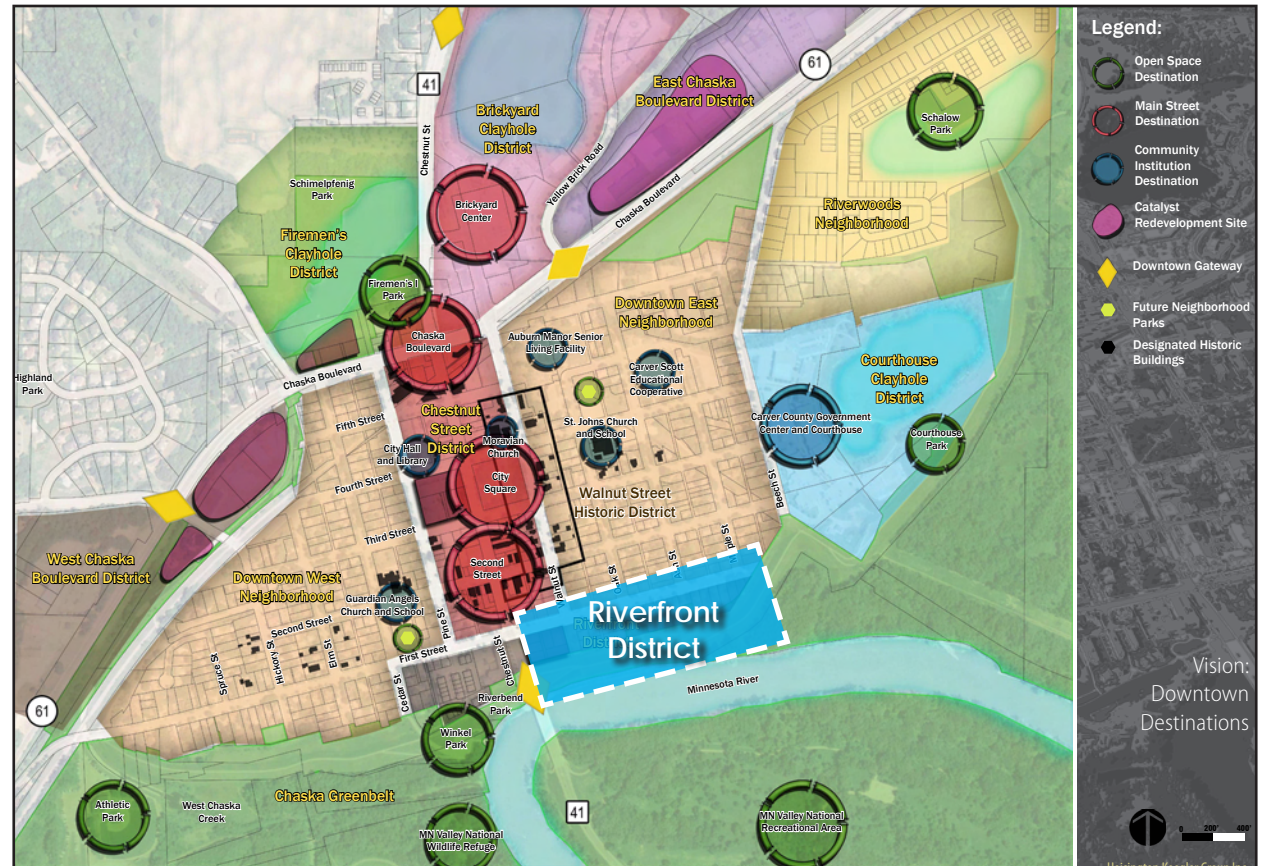


Figure 7-15. Riverfront District catalyst redevelopment site.

### Riverfront District - Scenarios A & B

Both scenarios feature high density senior housing on block 53, walk-up townhomes on blocks 50, 51 and 52, and lower levee park improvements and connections. Block 53 could include ground level retail or restaurant uses at the corner of Chestnut and First Streets. The dead-end street segments (Levee Lanes) are envisioned to become less street-like and more plaza-like, with special (pervious) paving treatment. As previously mentioned, blocks 50, 51 and 52 are seen as long-term potential sites for future housing density in downtown Chaska, if and when the City determines that is critical to the success of revitalizing downtown and its relationship to the river. The following is a breakdown of the proposed development program.

Figure 7-16.  
Redevelopment scenario A for Riverfront District catalyst redevelopment site.



Scenario A (Block 53 only)	Parking requirement = 120 spaces
Ground Story:	Retail/Restaurant (8,000 SF) Parking 120 spaces
2nd Story:	Residential (40 units)
3rd Story:	Residential (40 units)
4th Story:	Residential (40 units)
Structured parking provided = 120 spaces	
On-street parking provided = 22 spaces	
Surplus parking = 22 spaces	

Figure 7-17.  
Redevelopment scenario B for Riverfront District catalyst redevelopment site.



Scenario B (Blocks 50, 51 and 52)	
Walk-up courtyard townhomes	Residential (60 units)
Parking	Included with each unit

## B. Connections

### Introduction

Because streets occupy the largest amount of space in the public realm, they have a significant impact on how people experience downtown Chaska. Streets provide access to businesses and homes, convey utilities, and form the identity of the downtown environment. Since streets are shared by drivers, pedestrians, cyclists and parked vehicles, the design of the streets should address the needs of all users. A well-designed and maintained street system can help revitalize downtown Chaska in several ways including some of the following:

- » Create a greater sense of place and enhance community identity
- » Preserve and restore historically significant sites, buildings and streetscapes
- » Provide safer, more comfortable pedestrian routes to homes, schools, businesses and recreational destinations
- » Provide a more comprehensive and connected system of bike routes
- » Calm vehicle traffic
- » Provide additional on-street parking
- » Reduce greenhouse gas effects
- » Improve air and water quality
- » Enhance property values and create a more vibrant retail environment



Figure 7-18. Downtown Streets System Concept showing street design types for each street.

### Complete Streets

In order to achieve the goals above, the streets in downtown Chaska should be thought of as a system of "complete streets". Pedestrians, bicyclists, motorists and public transit users of all ages and abilities should be able to safely and comfortably move along and across the streets. In order to ensure safety and accessibility, streets must become more complete. Complete streets are roadways designed and operated to enable safe, attractive and comfortable access and movement for all users. They have the added benefit of addressing stormwater needs and enhancing air and water quality by providing green space as part of the streetscape.

## Streetscape Zones

It is useful to understand the different zones that make up the street since these zones will be mentioned in subsequent sections describing the street types in downtown Chaska. Each zone plays a role in the functioning of each street in downtown Chaska. The zones that make up the street include the following:

### *Pedestrian Zone*

The pedestrian zone is the portion of the streetscape that accommodates walking. Important design elements of the pedestrian zone include pavement materials, adequate width for movement in both directions, and protection from the sun and rain. Typically, the pedestrian zone is located at the outside of the street right-of-way, away from moving traffic and near the adjacent land uses.

### *Amenity Zone*

The amenity zone is the portion of the streetscape where amenities are located to enhance the pedestrian experience. The amenities could be as simple as street trees with a grass strip or more elaborate to include pedestrian lighting, seating, signage, planters, banners, trash receptacles, bicycle parking and enhanced landscaping. Typically, the amenity zone is located between the pedestrian zone and the vehicular travel zones, providing a safety buffer between people walking and people driving.

### *Parking Zone*

The parking zone is the on-street parking bay alongside the street. It can include parallel or diagonal parking spaces. On-street parking plays two important roles. One, it provides necessary convenient parking in front of shops and services in the downtown. Two, it provides a buffer between pedestrians and vehicular traffic. The parking zone is typically located outside of the traffic lanes, at the curb and gutter on each side of the street.

### *Travel Zone*

The travel zone makes up the traffic and bike lanes on the street. These lanes should provide adequate space for the safe movement of vehicles and bicycles on the street, without dominating the street width. Typically, vehicular travel lanes are located in the center of the street, while bicycle lanes are located between the vehicular lanes and the on-street parking or curb and gutter.

### *Median Zone*

The median zone is included on some streets, but not all. Typically, this zone is reserved for special types of streets such as highways, parkways, and other types of green streets. Median zone width may vary from street to street. Medians typically include landscape treatment with street trees and groundcover, but they may also be paved areas. Medians are typically located within the center of the street right of way.



*Ped | Amenity | Parking | Travel | Median | Travel | Parking | Amenity | Ped*

*Streetscape Zones shown above for an example street with a median.*

## Street Types

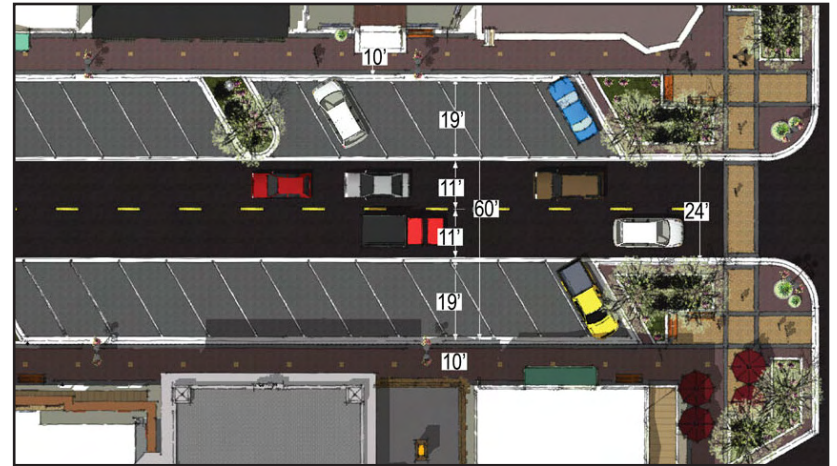
A hierarchy of eight (8) different street types are envisioned in Downtown Chaska, including Town Center Street, Commercial Edge Street, Trail Street, Signature Street, Residential Street, Levee Lane, Paseo, and Future Chestnuts Street/Hwy 41.

Each street type functions in different ways. Some streets function to carry high volumes of traffic, such as Chestnut Street/MN Highway 41. Some streets carry local traffic to homes, schools and businesses. Other streets define the retail character of Downtown Chaska and cater to the needs of the pedestrian, such as 2nd Street. Each street type is described in terms of its function, character, scale, and design elements on the following pages.

### Town Center Streets

Town center streets include those streets located within the historic commercial core of Downtown Chaska that provide access and convenient parking to shops, businesses and civic facilities located on Chestnut Street, 2nd, 3rd, 4th, and 5th Streets. These streets should be designed to balance the needs of automobiles and other modes of travel, with primary emphasis on the pedestrian environment. They should convey a sense of vitality and identity to the downtown retail environment and include a richly designed amenity zone. The following design considerations should be included in the design of the Town Center Streets:

- » *Historic character, scale and materials*
- » *Narrow travel lanes – 11 foot width is recommended*
- » *Diagonal on-street parking with permeable paving if feasible*
- » *Adequate width of the pedestrian zone – 10 ft. minimum width is recommended*
- » *Special paving in the pedestrian zone – brick or concrete pavers*
- » *Enhanced landscaping, raised planters, seating and signage at intersections*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*
- » *Pedestrian-scaled light pole fixtures*
- » *Street trees for shade where feasible*
- » *Trash receptacles and convenient bicycle parking*
- » *Building signage, awnings and shop windows*
- » *Minimal building setbacks*
- » *Small, informal plazas where feasible*



*Plan view of Town Center Street concept.*



*Bird's eye view of Town Center Street concept.*



*Ground level view of Town Center Street concept.*

## Commercial Edge Streets

Commercial edge streets are those streets that lie between the commercial core and the adjacent residential areas. This street type includes Pine and Walnut Streets in Downtown Chaska. They are unique in that they provide access to shops, businesses, civic facilities and residential properties. These streets should be designed to balance the needs of automobiles with the safety of pedestrians and other modes of travel. The commercial edge street should bump out at intersections to decrease the distance of pedestrian crossings and delineate the limits of on-street parking. The scale and design considerations on each side of the street should correspond to the adjacent land use. The commercial street edge should have a more urban character while the residential street edge should have a more residential character. The following design considerations should be included in the design of the Commercial Edge Streets:

- » *Historic character, scale and materials*
- » *Narrow travel lanes – 11 ft. width is recommended*
- » *Diagonal on-street parking (with permeable paving if feasible) on the commercial side of the street*
- » *Parallel on-street parking on the residential side of the street*
- » *Adequate width of the pedestrian zone – 10 ft. minimum width is recommended on the commercial side of the street with a 6 foot minimum width recommended on the residential side of the street*
- » *Special paving in the pedestrian zone (brick or concrete pavers) on the commercial side of the street*
- » *Enhanced landscaping, raised planters, seating and signage at intersections*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*
- » *Pedestrian-scaled light pole fixtures*
- » *Street trees for shade*
- » *Tree lawns on the residential side of the street*
- » *Trash receptacles and convenient bicycle parking on the commercial side of the street*
- » *Building signage, awnings and shop windows on commercial buildings*
- » *Minimal building setbacks on the commercial side of the street*



*Ground level view of Commercial Edge Street concept.*



*Bird's eye view of Commercial Edge Street concept.*



*Ground level view of Commercial Edge Street concept.*

## Trail Streets

Trail Streets coincide with the downtown trails plan. They include portions of Cedar St, Ash St, Beech St, Stoughton St, 3rd and 6th Streets. The trail streets are very unique, particularly within a small downtown urban environment. The trail street concept includes an 8 ft multi-use trail on one side of the street, with a standard 6 ft sidewalk on the other side. The multi-use trail offers residents the ability to walk and bicycle through downtown off-street. Trail streets are proposed to be narrowed from the existing 52 ft street cross section to 36 feet. The narrowing of the pavement section allows additional green space in the streetscape, which could be utilized to create raingardens to manage stormwater runoff. The trail street should bump out at intersections to decrease the distance of pedestrian crossings and delineate the limits of on-street parking. Multi-use trails should be distinguished from the standard sidewalks by using a different concrete color and/or pattern. They should also be maintained by City staff as they would become a part of the City's trails system.

There are two types of Trail Streets recommended for downtown Chaska. Trail Street A has on-street parallel parking on each side of the street, while Trail Street B has one side with on-street parallel parking and one side with on-street diagonal parking. Locations for each type are indicated on the Downtown Streets System Concept Diagram. Trail Street B offers the opportunity to include more on-street parking in areas where demand is higher, such as near Guardian Angels and St. John's. Basic design principles for each type are identical other than the parking configuration. The following considerations should be included in the design of Trail Streets:

- » *Historic character, scale and materials*
- » *Narrow travel lanes – 11 ft. width is recommended*
- » *Parallel or diagonal on-street parking (depending on type of Trail Street)*
- » *Adequate pedestrian zone – 6 ft. minimum width recommended for sidewalks*
- » *Sidewalks should be detached from the curb with a tree lawn in between*
- » *Multi-use trails – 8 ft. width recommended with identifiable color/finish on trail*
- » *Trail system signage*
- » *Enhanced landscaping at intersections*
- » *Pedestrian-scaled light pole fixtures at intersections*
- » *Street trees for shade*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*



*Plan view of Trail Street B concept.*



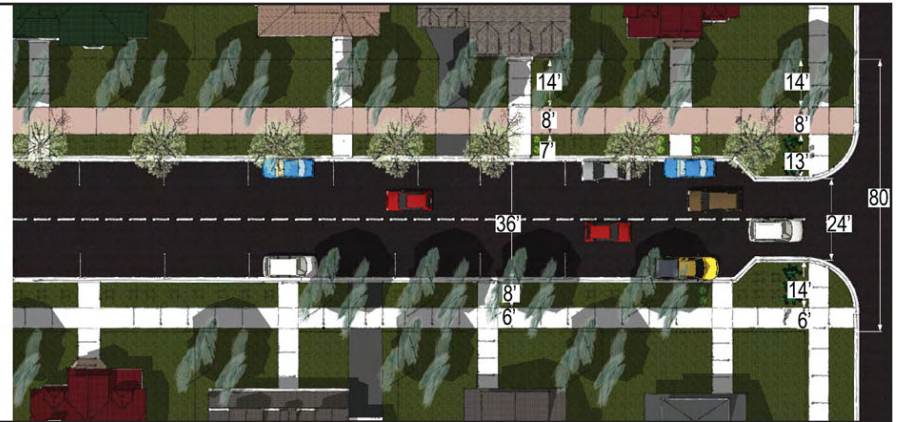
*Bird's eye view of Trail Street B concept.*



*Ground level view of Trail Street B concept.*



Plan view of Trail Street A concept with trail next to parking.



Plan view of Trail Street A concept with street trees between trail and parking.



Bird's eye view of Trail Street A concept.



Bird's eye view of Trail Street A concept.



Ground level view of Trail Street A concept.



Ground level view of Trail Street A concept.

## Signature Streets

Signature Streets in the downtown provide a sense of unique character, orientation and east/west movement between the east and west residential neighborhoods on either side of the Chestnut Street commercial corridor. This street type includes 2nd Street, west of Pine Street and east of Walnut Street. It also includes 4th Street, west of Pine Street and east of Walnut Street. These streets are unique in that they include a small planted and/or paved median. They connect the neighborhood churches/schools of Guardian Angels and St. Johns with other downtown destinations. At these institutions, the street should be adapted to allow safe and efficient bus loading/unloading and pedestrian traffic. The signature street should bump out at intersections to decrease the distance of pedestrian crossings and delineate the limits of on-street parking. The City has already begun a section of this concept on 4th Street, west of Pine Street. This should set the design tone for the rest of the signature street sections. The following design considerations should be included in the design of the Signature Streets:

- » *Historic character, scale and materials*
- » *Narrow travel lanes – 11 ft. width is recommended*
- » *Parallel on-street parking*
- » *Adequate width of the pedestrian zone – 6 ft. minimum width is recommended*
- » *Sidewalks should be detached from the curb with a tree lawn in between*
- » *Special paving in the high volume pedestrian zones – brick or concrete pavers, particularly at school and church crossings*
- » *Enhanced landscaping at intersections*
- » *Pedestrian-scaled light pole fixtures*
- » *Street trees for shade*
- » *Planted medians with ornamental trees and enhanced groundcover planting*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*



*Bird's eye view of the Signature Street concept.*



*Bird's eye view of the Signature Street concept.*



*Ground level view of the Signature Street concept.*

### Residential Streets

Residential Streets are the typical street in the residential districts of Downtown Chaska. They provide access to homes of downtown residents. Residential streets are proposed to be narrowed from the existing 52 foot street cross section to 36 feet. The narrowing of the pavement section allows additional green space in the streetscape, which could be utilized to create raingardens to manage stormwater runoff. Sidewalks should be detached from the curb and parallel on-street parking should be provided. The residential street should bump out at intersections to decrease the distance of pedestrian crossings and delineate the limits of on-street parking. The following design considerations should be included in the design of the Residential Streets:

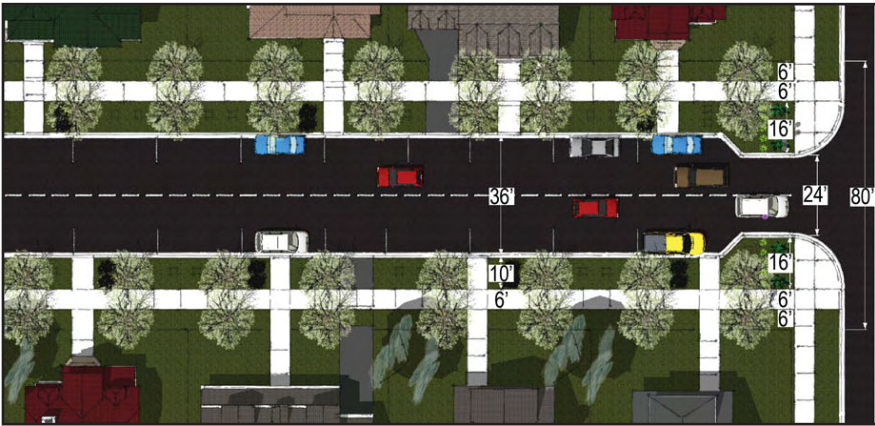
- » *Historic character, scale and materials*
- » *Narrow travel lanes – 11 ft. width is recommended*
- » *Parallel on-street parking*
- » *Adequate width of the pedestrian zone – 6 ft. minimum width is recommended*
- » *Sidewalks should be detached from the curb with a tree lawn in between*
- » *Enhanced landscaping at intersections*
- » *Pedestrian-scaled light pole fixtures at intersections*
- » *Street trees for shade*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*



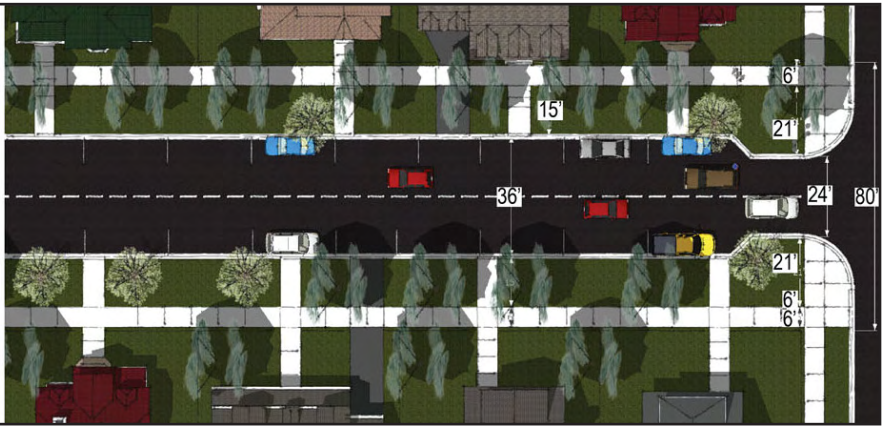
*Ground level view of Residential Street concept.*



*Ground level view of Residential Street concept.*



*Plan view of Residential Street concept.*



*Plan view of Residential Street concept.*

## Levee Lanes

Levee Lanes are those street sections south of 1st Street. Each is a dead end street and they may provide access to the levee park. The levee lane at Cedar Street provides access to the Minnesota River, so this may be a unique circumstance requiring a different approach, however since the rest of these streets carry very low volumes of automobile traffic, they should be thought of as a pedestrian domain, balancing the needs of pedestrians and auto traffic. Levee lanes should be as narrow as possible and include parallel parking on each side of the street. They should also include pervious pavement systems. The following design considerations should be included in the design of the Levee Lanes:

- » *Historic character, scale and materials*
- » *Narrow travel lanes*
- » *Parallel on-street parking*
- » *A street cross-section totaling 32 ft.*
- » *Adequate width of the pedestrian zone – 6 ft. minimum width is recommended*
- » *Sidewalks should be detached from the curb with a tree lawn in between*
- » *Street trees for shade*
- » *Pervious pavements should be considered if feasible*



*Bird's eye view of Levee Lane concept.*



*Bird's eye view of Levee Lane concept.*



*Plan view of Levee Lane concept.*



*Plan view of Levee Lane concept.*

**Paseos**

Alleys in the commercial core, between Pine and Walnut Streets, should be considered as part of the pedestrian walkway system in downtown Chaska. These are referred to as a “Paseo”. They run north/south, parallel to Chestnut Street and provide alternative pedestrian routes off the street system, allowing safe and convenient walks from parking areas to businesses and shops. A paseo should be designed to add vitality and create interest along the alleyways. The following design considerations should be included in the design of the Paseos:

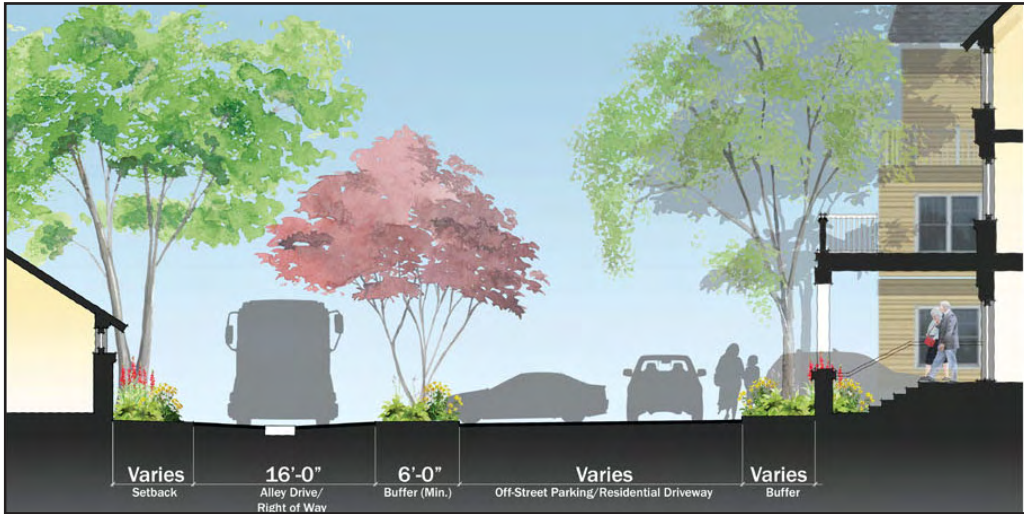
- » *Historic character, scale and materials*
- » *16 ft. alleyway for auto/pedestrian movement*
- » *Buffer plantings where feasible*
- » *Minimal setbacks*
- » *Special pedestrian lighting*
- » *Special pavement systems*
- » *Seating, trash receptacles and bicycle racks*



*Precedent image of Paseo concept.*



*Precedent image of Paseo concept.*



*Cross section of Paseo concept.*

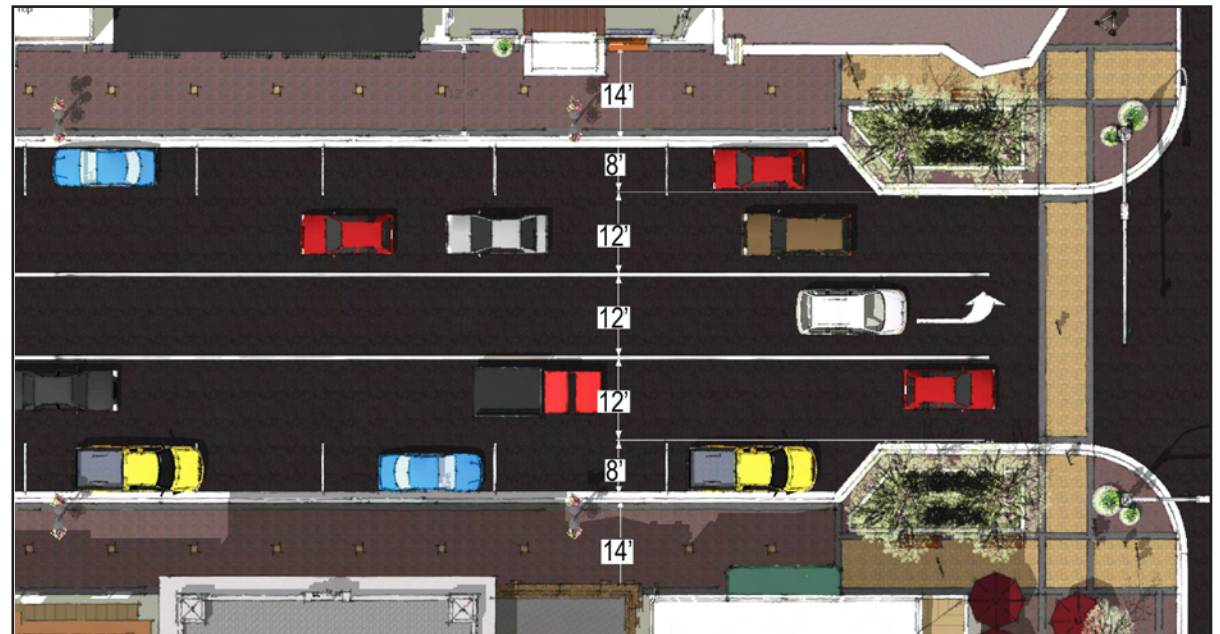


*Cross section of Paseo concept.*

## Future Chestnut Street/MN Highway 41

The desired future Chestnut Street/MN Highway 41 is contingent upon many factors, including MnDOT approval, a reduction in projected traffic counts due to a new MN Highway 41 river crossing, and a change in the type of traffic from regional (through traffic) to downtown destination type traffic. If the above occurs, the future design of Chestnut Street might offer a more pedestrian-friendly streetscape than currently exists. The concept is to reduce the travel lanes to three 12 foot lanes and include parallel on-street parking. This would allow more space in the amenity/pedestrian zones on both sides of the street. The results of the redesigned streetscape would make Chestnut Street a more vibrant environment for pedestrians and could enhance the downtown retail environment. The following design considerations should be included in the design of the future Chestnut Street/MN Highway 41:

- » *Historic character, scale and materials*
- » *12 foot travel lanes are recommended*
- » *Parallel on-street parking*
- » *Adequate width of the pedestrian zone – 14 ft. minimum width is recommended*
- » *Special paving in the pedestrian zone – brick or concrete pavers*
- » *Enhanced landscaping, raised planters, seating and signage at intersections*
- » *Bumpouts at intersections to decrease distance of pedestrian crossings and delineate limits of on-street parking*
- » *Pedestrian-scaled light pole fixtures*
- » *Street trees for shade where feasible*
- » *Trash receptacles and convenient bicycle parking*
- » *Building signage, awnings and shop windows*
- » *Minimal building setbacks*
- » *Small, informal plazas where feasible*



## C. Parks and Open Spaces

### Introduction

A strong park, recreation and open space system is integral to the high quality of life Chaska residents enjoy. While many socio-economic changes have taken place in recent years, the need for parks, recreation and open space conveniently accessible to Chaska residents has not changed. Indeed, increased leisure time and greater awareness of the benefits of maintaining good health has placed even more emphasis on recreation than in the past.

Changes in employment and living patterns are altering patterns of parks and recreation use and are creating more demand at new times of the day. Notable trends include a shorter workweek, flexible work hours, home offices, and two-income households. For example, use of parks during day hours by mothers with toddlers has decreased, but park use at day's end by young families has increased.

More and more, people are drawn to parks and trails, public gathering areas, tree-lined streets and sidewalks, special park features and sports facilities. Current trends indicate a preference for more trails, better connections to parks and more natural open spaces. Communities that preserve and enhance their scenic, ecological and recreational assets have a competitive edge over those that do not.

### Downtown Chaska Parks and Open Spaces

Downtown Chaska lies within the Minnesota River Valley and adjacent to the 14,000-acre Minnesota River Valley National Wildlife Refuge and the 5,400-acre Minnesota Valley State Recreation Area. The Minnesota Valley National Wildlife Refuge is one of only a handful of urban national wildlife refuges in the nation, a place where coyotes, bald eagles, badgers, and beavers live next door to three million people. The Minnesota Valley State Recreation Area is a member of the Minnesota State Park system. Together, these open spaces offer unprecedented access to nature-based resources, trails and recreation.

Downtown Chaska is physically and visually separated from the river by a 30-foot high levee, constructed to protect homes and businesses downtown from seasonal flooding of the Minnesota River. An existing auto access, parking and public boat launch connects downtown to the river. Greater measures should be taken to physically and visually connect downtown Chaska to the river and the abundance of nature-based national and state resources and recreational lands.

Other significant natural and manmade features form parks and open spaces in downtown Chaska, including the town's historic clayholes and creeks. The areas around each clayhole have been preserved and/or designed as public parks and open spaces. Fireman's Park and Clayhole, together with Brickyard Clayhole, form an open space gateway into downtown from the north, along Highway 41. Fireman's Park and Clayhole has historic significance for both its brickyard past and its place in downtown Chaska as a public park.

Courthouse Lake, another clayhole/open space, located just east of the County Courthouse attracts





Figure 7-19. Chaska Downtown Parks, Open Space and Trails System Concept.

downtown residents, workers and visitors to hike its trails and picnic in its open spaces.

Chaska's Athletic Park, located between the Minnesota Valley National Wildlife Refuge and the town levee, plays host to Chaska's premier ball park and home to the Chaska Cubs. The Athletic Park is currently being planned for additional park uses, circulation and parking improvements.

At the center of downtown lies Chaska's City Square Park. City Square Park lies along Chestnut Street, between 3rd and 4th Streets. It is one block in size and is historically significant. It contains several historic burial mounds from the "Mound Building" natives of the area. As Chaska's most significant public space, it is the place for annual social gatherings and events. The center of the park includes a gazebo. Large shade trees dot the park.

Downtown Chaska also possesses a few small neighborhood parks (Hickory Park and Fireman's II Park), however, these are difficult to find and are not adequate to serve downtown's current population. There is a need for additional neighborhood parks in both the west and east residential districts. The plan identifies a few options for these additional neighborhood park spaces.

The existing trail system in downtown Chaska includes trails and trailheads that provide access into the nearby wildlife and recreation areas and along the levee. The Downtown Master Plan proposes additions to the trail and trailhead system within downtown with connections to outlying areas surrounding downtown. There is a tremendous opportunity to create a regional destination in Chaska as the "gateway" into the Minnesota River Valley and its nature-based recreational offerings including hiking, bird-watching, paddling sports, fishing and bicycling.

## Park, Recreation and Open Space Classification System

The National Recreation and Park Association (NRPA) has developed a classification system for specific kinds of parks, recreation and open space facilities. It addresses function, design, use, size, resource characteristics and preferred location.

Chaska has adapted a similar system to the NRPA's - one that more closely reflects the City's unique development patterns and needs. The following table describes the City's classifications for parks, recreation and open spaces according to use, service area, size, site attributes and site location.

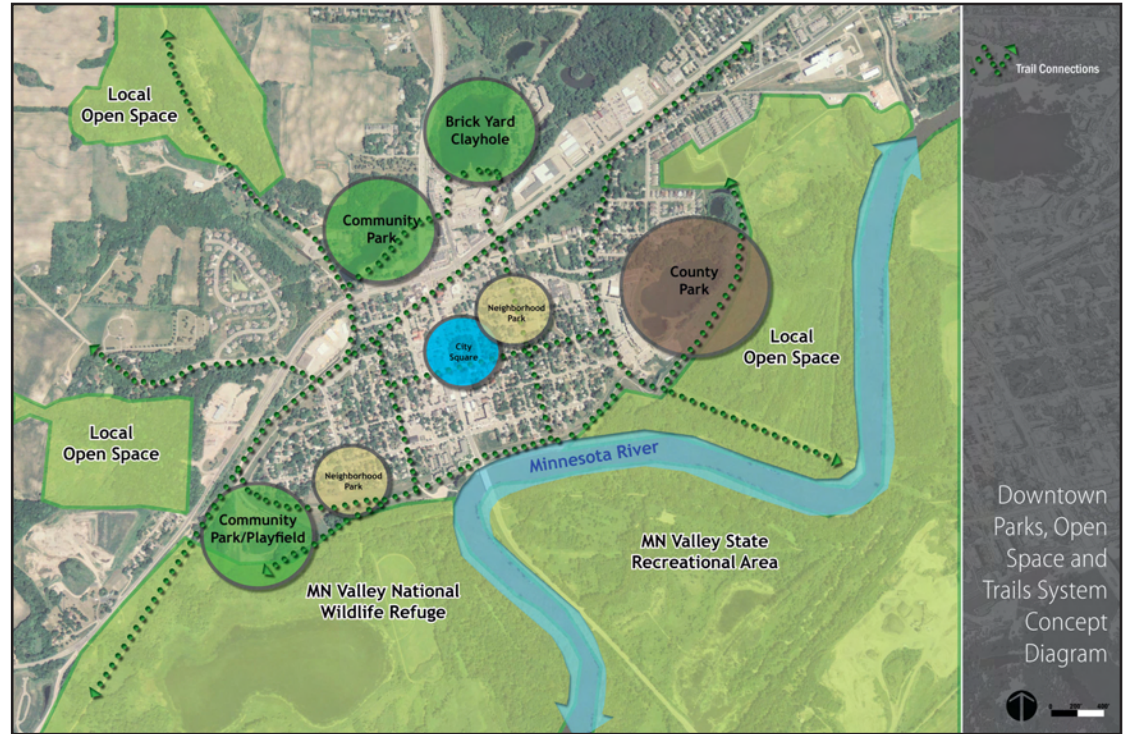
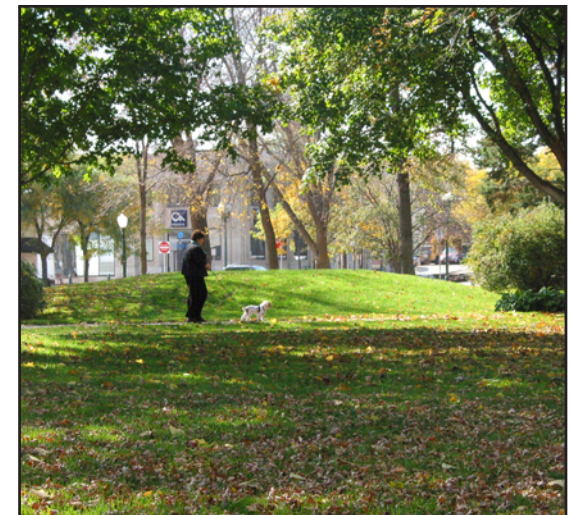


Figure 7-20. Chaska Downtown Parks, Open Space and Trails System Concept.



COMPONENT	USE	SERVICE AREA	SITE SIZE	SITE ATTRIBUTES	SITE LOCATION
<b>LOCAL FACILITIES</b>					
Mini-Park	Specialized facilities that serve a concentrated or limited population or specific group such as tots or senior citizens	Less than ¼ mile radius	Less than one acre		May be in residential neighborhoods or commercial centers that currently lack convenient access to a neighborhood park / playground or as part of a planned residential development
Neighborhood Park / Playground	Area for active recreational facilities such as field games, play court games, crafts area, play apparatus area, skating, shelter, open play area	¼ to ½ mile radius (neighborhood)	3-15 acres	Physical geography suited for intense development	Easily accessible to neighborhood population by foot or bike; centrally located with safe and convenient walking and biking access; may be part of a Community Park or Playfield
Community Playfield	Area for active recreational facilities such as athletic fields for youth and adult league use, and could include neighborhood park uses	3 -5 neighborhoods or whole community	25 – 50 acres	Physical geography suited for intense development	Easily accessible by motor vehicle; edge of neighborhood or in non-residential area
Community Park	Area of natural or ornamental quality for outdoor recreation such as walking, viewing, sitting, picnicking; could have some field and court games; could include neighborhood park uses	3 -5 neighborhoods or whole community	25 – 50 acres	Affords natural features with varied physical landscape interest	Proximity to community facilities and natural resources
County Park	Area of natural or ornamental quality for outdoor recreation such as walking, viewing, sitting, picnicking; could have some field and court games	County	25 – 50 acres	Affords natural features with varied physical landscape interest	Proximity to community facilities and natural resources and/or where natural resource occurs
Conservancy Lands	Area of natural quality such as water-courses and wetlands which are preserved for environmental or aesthetic benefits to the community, and/or because of the negative environmental or economic effects of development on them	Community and County	Variable, based on extent of natural resources	Natural resources that merit preservation and would be negatively affected by development	Where natural resource occurs
<b>REGIONAL FACILITIES</b>					
Regional Park	Area of natural or ornamental quality for nature-oriented outdoor recreation such as picnicking, boating, fishing, swimming, camping, and trail uses	3 – 5 communities *	200 – 500 acres (100 acre minimum)	Complete natural setting contiguous to water bodies or watercourses where possible	Where natural resource occurs – particularly water
Regional Park Reserve	Area of natural or ornamental quality for nature-oriented outdoor recreation such as viewing and studying nature, wildlife habitat, conservation, swimming, picnicking, hiking, boating, camping, and trail uses	County, multi-county area	1000 + acres; sufficient area to encompass the resource envisioned for preservation	Diversity of unique resources, such as topography, lakes, streams, marshes, flora, fauna	Where natural resource occurs
<b>LOCAL OR REGIONAL FACILITY</b>					
Special Feature	Area that preserves, maintains and provides specialized or single-purpose recreational activities such as golf course, nature center, marina, zoo, conservatory, arboretum, display gardens, arena, gun club, downhill ski area, and sites of historic or archeological significance		Specific standard applicable to desired feature	Appropriate to particular special feature	Where most advantageous for the special feature and the overall park system

Table 7-1. Chaska Classification System for Park, Recreation and Open Spaces.

# Parks and Open Spaces Recommendations and Strategies

The following specific recommendations and strategies are intended to restore and enhance the quality of downtown Chaska’s parks, recreation, trails and open space system.

## Key Recommendations/Strategies:

### Fireman’s Park

- » Enhance the community park at Chestnut Street/Hwy 41 and Chaska Boulevard/Hwy 61 including the area of Fireman’s Park and Clayhole, and Schimelpfenig Park
- » Incorporate revenue generating land uses into the park for year-round interest, including restaurants, performing arts facilities, recreation and leisure uses.
- » Incorporate Chaska brick into the design of structures and park elements
- » Park and associated structures should address gateway/identity issues
- » Program events in the park for each season of the year
- » Design and programming should address historical aspects of the park and it’s place in the City’s history
- » Design the park to accommodate large community gatherings and events
- » Program ideas include: ice skating, boat races, paddle boats, music in the park, plays/theater, restaurant, warming house, picnicking, beach, nature-based play areas, splash park, gardens, lawns, patios, parking, etc.

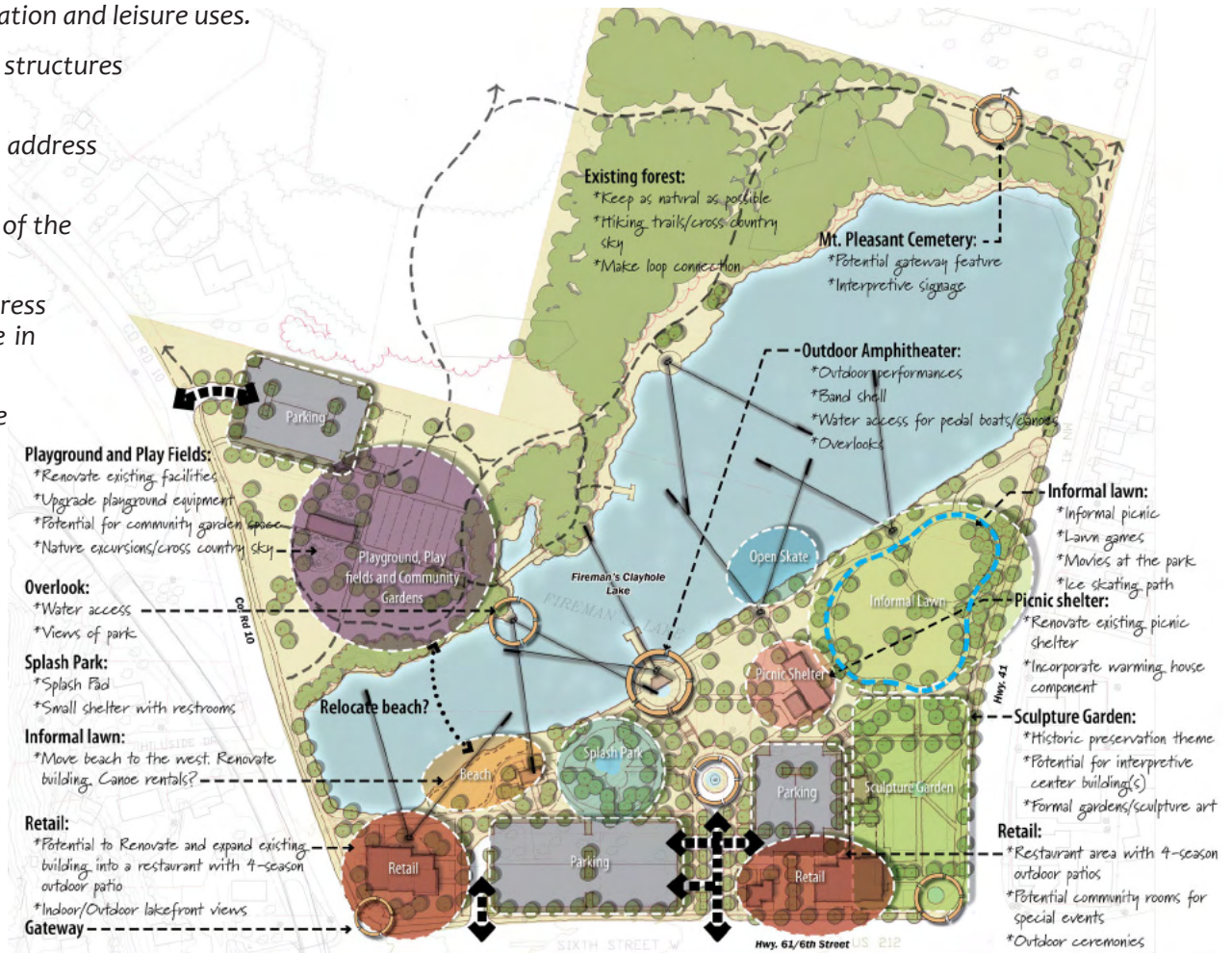


Figure 7-21. Fireman’s Park Concept Scenario B.

## Athletic Park

- » Develop a master plan for Athletic Park
- » Enhance the baseball experience
- » Include additional programmed park uses – utilizing additional site area
- » Improve site access/entry into park
- » Improve parking
- » Improve and enhance connections to surrounding open space and trails systems

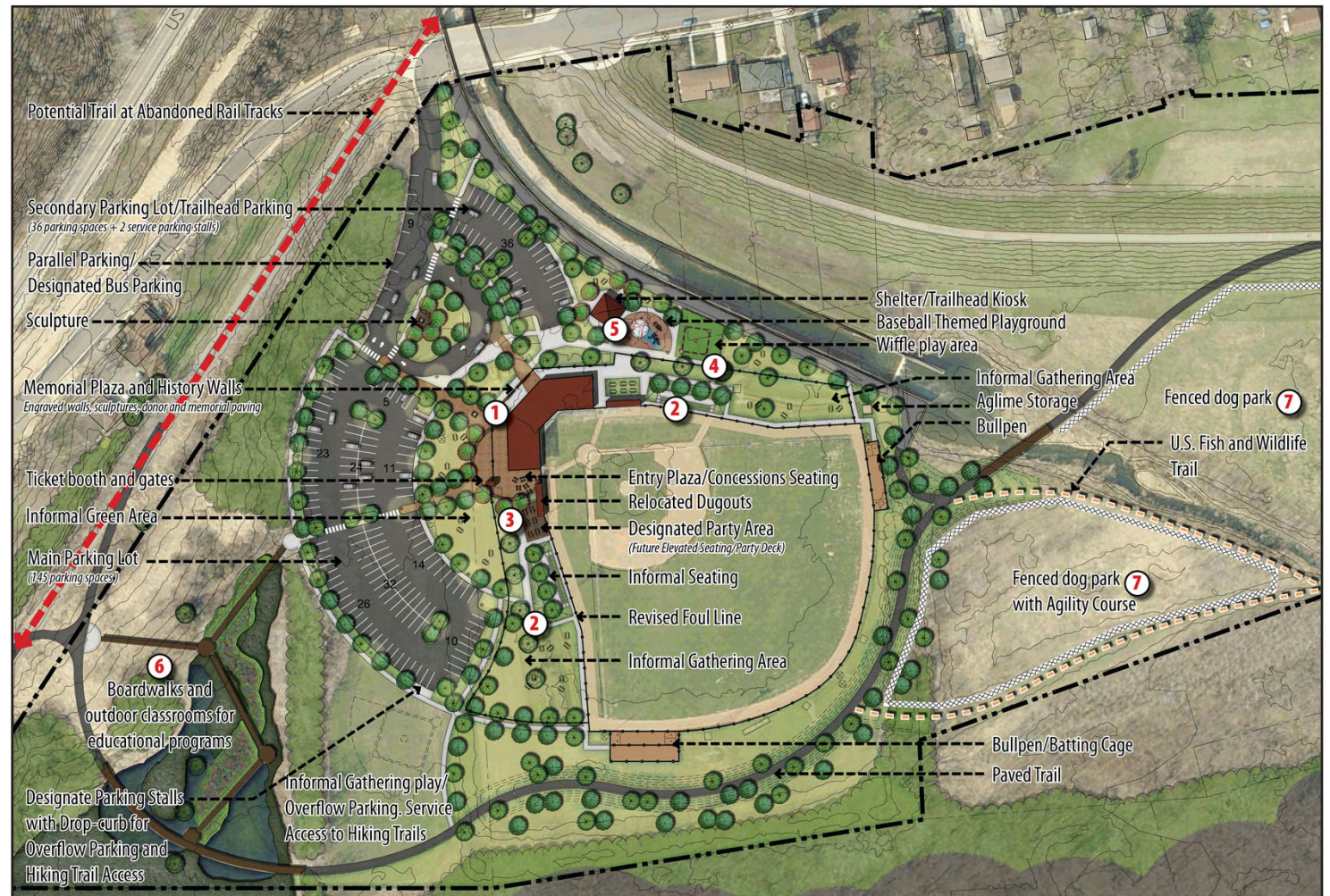


Figure 7-22. Athletic Park Master Plan Concept.

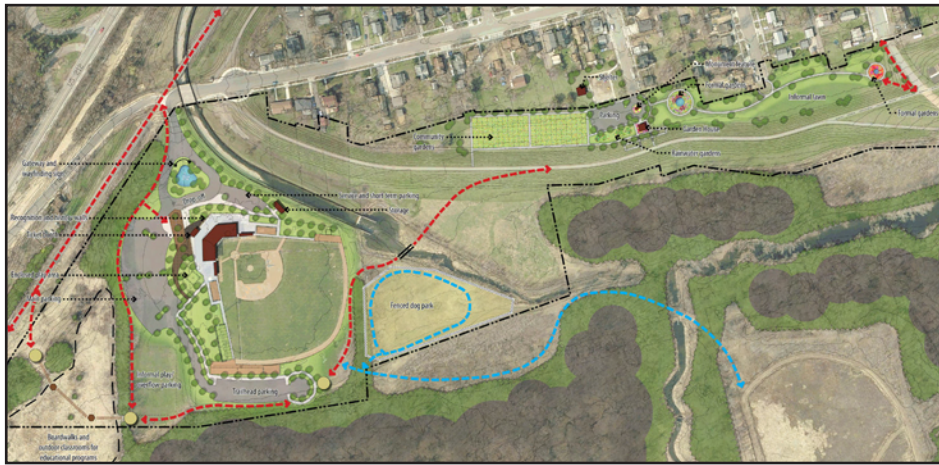


Figure 7-23. Chaska Athletic Park Master Plan - Alternative 1.



Figure 7-24. Chaska Athletic Park Master Plan - Alternative 2.

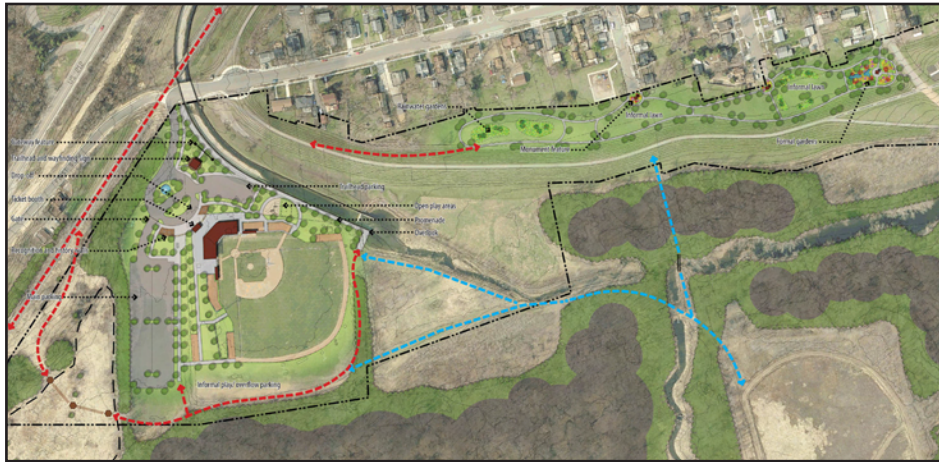


Figure 7-25. Chaska Athletic Park Master Plan - Alternative 3.

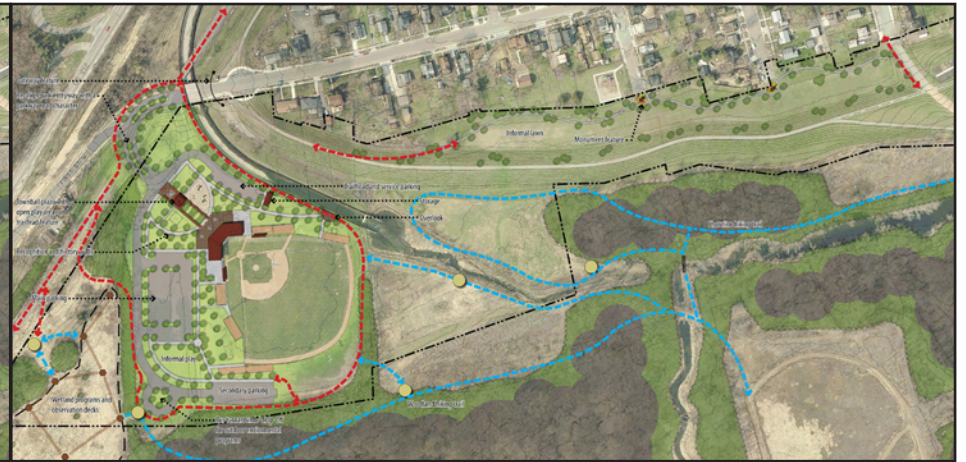


Figure 7-26. Chaska Athletic Park Master Plan - Alternative 4.

## Neighborhood Parks

- » Identify neighborhood park sites on the east and west sides of Chestnut Street (Hwy 41). Several candidate sites are identified on the plan.
- » Recommended park site east of Chestnut – north of St. John’s Church and school, between 4th and 5th Streets and Oak and Ash Streets.
- » Recommended park site west of Chestnut – expand the Hickory Park and the River Levee into a single neighborhood park.
- » Develop a plan for each neighborhood park to address neighborhood park needs (i.e. games, play courts, play apparatus, skating, shelter, open play area, etc.)
- » Design clearer distinctions between public and private spaces on the River Levee site, between the levee and the homes that line the levee.



## Mini-Parks/Plazas

- » Identify potential mini-parks and city plaza sites. Several are identified on the Parks, Trails and Open Space Plan.
- » Mini-park and plaza candidate sites west of Chestnut:
  - Guardian Angels Church and School sites
  - Triangle site at 4th and Elm Streets
  - Plaza site on Chestnut between 2nd and 3rd Streets
- » Mini-park and plaza candidate sites east of Chestnut:
  - Site east of St. John’s school site
  - Turned-back power line easement through neighborhood
  - Fireman’s Park II

## **Special Features**

- » *Develop a trailhead facility at City Square (signage, parking, shelter, etc.)*
- » *Develop a plan for special features along the River Levee (i.e. community gardens, park shelters, seating, sculpture, etc.)*
- » *Develop a plan for Fireman's I Park that will include identity elements/structures that play on the history of the City of Chaska and this special site.*

## **Trails/Connections**

- » *Create a plan for off-street trails on Cedar, Ash and 3rd Streets*
- » *Improve connections between park destinations and other Downtown destinations via trails and sidewalks*
- » *Incorporate an alley (paseo) plan for alleys indicated on the plan*
- » *Convert the rail line to a trail system*
- » *Incorporate trailheads and information kiosks where indicated on the plan*
- » *Incorporate better signage and wayfinding to trail systems as part of the streetscape design work for downtown Chaska.*
- » *Develop a heritage walk/promenade along the levee.*

